

50 YEARS

€ 7.50 (D), € 7.50 (A), CHF 13.50 (CH), \$ 15.00 (US)



Depesche

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THE MAGAZINE FOR LGB ENTHUSIASTS

ISSUE 1/2018



A legend turns 50

The RhB anniversary loco marks the important LGB birthday

SPECIAL FEATURE: 50 YEARS OF LGB

- The success story of Lehmann-Gross-Bahn
- LGB – superlative and extraordinary
- The 2018 anniversary trips

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DEAR READERS,



Your contacts at LGB: Silvia Römpp (LGB Club) and Andreas Schumann (Product Manager LGB).

Lehmann-Gross-Bahn is celebrating its 50th anniversary. By the time you read this, the half-century will already have set a large part of the agenda for new year. That is because this Ge 4/4 II anniversary loco of the RhB was unveiled to the general public in prototypical scale on January 12 in Landquart. It will operate throughout the LGB anniversary year on the routes of the Rhaetian Railway ... and perhaps in 1:22.5 scale on your layout during the new garden railroad season. We are delighted with the success of this great gauge, and with the loyalty of LGB fans! For this reason, 2018 will see the introduction of numerous new models that recall the dynamic history of the Richters' garden railroad. Alongside this truly wonderful Ge 4/4 617 "Ilanz", the factory train with the Lenz type i steam loco 99 5606 from Saganer Strasse in Nuremberg and the gold-painted Stainz loco complete with five anniversary cars, you can look forward to many other birthday surprises. Apart from this, the long-awaited G 4/5 steam loco of the RhB will also be making its way to garden railroads in 2018. It is loco number 106, which entered service in Switzerland 112 years ago. G-gauge fans can also look forward to a genuine Swiss monster: the Xrot 9213 steam rotary snowplow. The finely detailed metal model made of brass and die-cast metal will carry on the tradition of the earlier LGB metal models. In this anniversary year, we will also be hosting yet another competition for the most beautiful garden railroad layout. Let yourself be inspired by the top layout of 2016 winner Dieter Schubert, and get the entry forms! See page 13 for details. And now, we hope you enjoy reading this issue!

SILVIA RÖMPP

ANDREAS SCHUMANN

LGB Depesche has also been in existence for 49 years. Each issue in the anniversary year will include eight extra pages of information.

50
Jahre | Years



6 The Ge 4/4 II "Illanz" goes for LGB in 2018.



1968-2018: 50 YEARS OF LGB

THE RAILROAD FOR INDOORS AND OUTDOORS **33**
Each issue of LGB Depesche during the anniversary year will include eight additional pages of history and stories to mark LGB's 50th birthday.

MODELS AND LAYOUTS

TOP MODEL **6**
The LGB anniversary loco has been running on the tracks of the Rhaetian Railway since January 12th. Naturally, the Ge 4/4 II 617 "Illanz" is also available as a model. Here it is.

TOP LAYOUT **14**
Dieter Schubert is the winner of the reader's competition "Best Garden Railroad 2016". His garden rail-

road is a G-gauge paradise packed into just 47-27/32 square yards.

THE MOVIE STAR AMONG STEAM LOCOS **22**
During its 115-year life, the G 3/411 previously starred in the 1952 movie "Heidi", from which it takes its current name. The brand-new model of the LGB steam loco replicates its appearance in era VI, as can be seen in Switzerland today.

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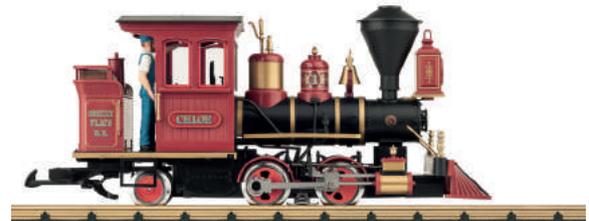
NEW ACCESSORIES **30**
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26 The Richter mail car, the G 4/5 106 of the RhB, the RhB Xrot 9213 steam rotary snowplow and lots of other models are the new items for the International Toy Fair in 2018.



48 "Heidi" is 115 years old, but thanks to good maintenance, oil firing and an electric pre-heating system, every bit as modern as its brand-new LGB counterpart fitted with an mfx/DCC decoder.



13 Your specialist dealer has plenty of steam locos: The former sugar cane loco "Chloe", the Saxon Vik and the DEV loco "Spreewald" are waiting for you. There is also plenty afoot on the diesel front!

Pola also has new items in stock: Tools in the wood store.

CLUBS MAKE THE HOBBY MORE FUN 32
Stefan Kühnlein knows how giant red rotary plow wheels and communal meals can form strong bonds between garden railroad fans from around the world.

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Ulrike and Jörg Pfeifle run a drugstore and, since 1991, a shop window railroad during the Christmas period with a different theme each year. It simply has to be seen.

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Karsten Speyer wants to build a home fit for the LGB trademark steam loco "Stainz" - with coal bunkers, a water-crane and everything else that a small steam loco needs.



33 Extra for you: An eight-page anniversary section in every issue of Depesche 2018.

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A mighty loco for the little railroad

The Rhaetian Railway anniversary loco "50 years of LGB"

The LGB garden railroad made its first appearance at the International Toy Fair 1968 in Nuremberg – 50 years ago. Since then, it has made its way into countless gardens and model railroad rooms. Just 10 years later, the frequently expressed wish for rolling stock of the Rhaetian Railway was fulfilled, initially with the Crocodile, one of the most famous RhB locos. Following the great success of this loco, the model of the Ge 4/4 II was introduced several years later and – like the Crocodile – is still available in a steady stream of new



An unusual sight today: The SLM-built Ge 4/4 II "Ilanz" delivered in 1973 featured the typical green paint scheme and round headlights.

variants. The introduction by the Rhaetian Railway of several anniversary locos to celebrate various route anniversaries led to the idea of realizing something similar to mark the 50th anniversary of the LGB garden railroad. No sooner said than done ... actually it wasn't quite that easy – this wasn't intended to be just a "normal" advertisement loco. The aim was to come up with a design that conveyed the association of LGB with the life-size prototypes and the landscapes in which the Rhaetian Railway operates on the one hand and that highlighted



the major event – the 50th birthday of the LGB – on the other. Last but not least, the design had to blend with the external shape of the loco. In terms of color, there was also a great deal to consider – from the basic color of the familiar LGB red, to the green and white stripes and the gray locomotive frame. After numerous experiments and optimizations – always in consultation with the responsible employees at the Rhaetian Railway – the colleague in charge of design at LGB managed to come up with a design that combines all of these wishes. Set against

Now RhB is advertising for LGB: With its anniversary loco “50 years of LGB”

the backdrop of the Bernina Range in the Grison Alps in the evening light are four models of the anniversary loco, arranged exactly alongside one another facing the viewer. The LGB logo and the lettering “50 Jahre LGB” (50 years of

LGB) – displayed in German, Italian and Romansh to reflect the region’s heritage – round out the image, so that loco fits seamlessly into the range of other RhB anniversary locos.

Now – once the design was complete – the second part of the job began. This involved finding a loco for which renovation was scheduled to coincide more or less with the end of 2017. One request made by LGB was, if possible, to select a loco with a road number that had yet to appear on a model. Following a thorough inspection of documents at the RhB ►

and at LGB, the choice fell to No. 617 bearing the name and coat of arms of the town of Illanz, also known as the first town on the Rhine – or Anterior Rhine to be precise.

The prototype of this loco was delivered by SLM in 1973, still in the green paint scheme typical of the time and sporting round headlights, a metal coat of arms bolted on and chrome-plated numerals and letters for the road number and railroad initials. It was one of a series of ten locos, all of which entered service in 1973. Since the locos proved to be exceptional performers, a second series of a further 13 locomotives was delivered in 1984, with the result that this loco type remains the largest loco series of the RhB today with a total of 23 units. Naturally, the loco was modernized at various times during its 45 years of service to date. The most important changes involved replacing the control technology and the semiconductor modules, although this was not visible externally. Changes that were clearly visible included the switch in 1984 to the red RhB paint scheme still in use to this day, and the installation starting in 2003 of the rectangular headlights, which were also fitted to all other locos in this series. The reason for this

change is actually very simple: Since it did not make economic sense to develop dedicated headlights for the series of 23 locos, the manufacturer SLM in Winterthur simply fitted standard truck headlights that were available at the time. This is why the locos had a high beam setting for the headlights from the outset, just like that of a truck. However, since a loco has a much longer service life than a truck, the time came when replacement headlights were no longer available. Expensive individual parts purchased on the second-hand market were also not a solution, so another solution had to be found: As an entirely pragmatic response, truck headlights that are cur-

RhB electric loco 617 "Illanz"



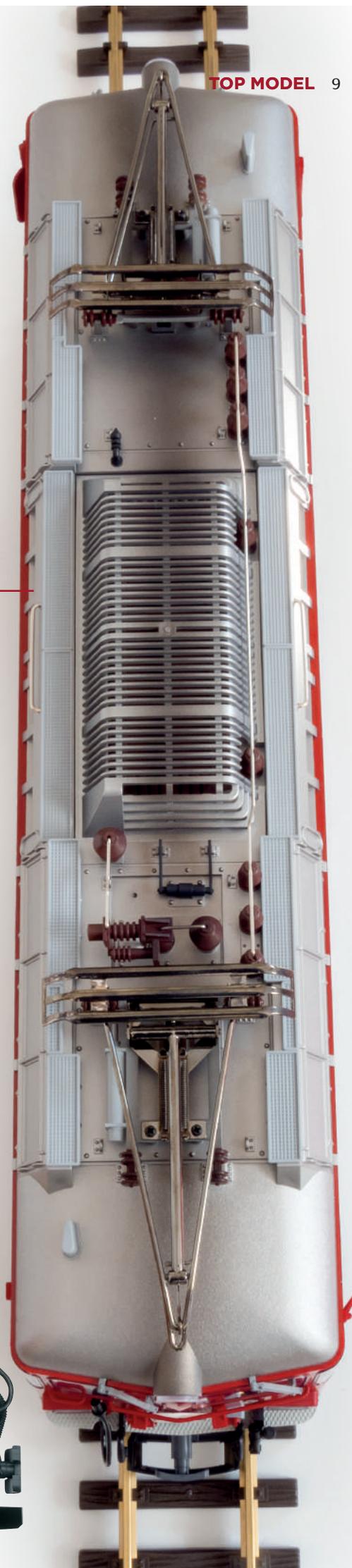
rently available were used. Admittedly, they are rectangular, but offered the advantage of allowing the red tail-lights to be installed in the turn-signal housing. This certainly altered the locomotive's face, but the typical SLM appearance, as can also be seen on the Re 4/4 II or Re 6/6 of the SBB, remains unchanged.

The chrome-plated numerals and letters were gradually replaced over the years with suitable adhesive letters. The first ten locos have retained their metal coats of arms, while those of the second series bore glued-on coats of arms from the start. Other additions include the second rear-view mirror, the train radio antenna as well as additional sockets on the front end. Loco No. 617 also went through a rather unusual experience during its lifetime: In 1984, an avalanche destroyed a bridge near Disentis on the railroad route to Chur, forcing the closure of the entire line. Several locos and cars were trapped in Disentis, including our No. 617. However, since the loco was urgently required on the rest of the RhB network, the RhB decided to transport the loco over the Oberalp Pass to Brig, where it was loaded onto an SBB truck and hauled to Landquart. With the help of the two HGm 4/4 hybrid locos of the Furka Oberalp Railway, the loco was driven over ►

HIGHLIGHTS

RhB electric loco Ge 4/4 II "50 years of LGB",
Era VI, item 28443

- 1 Prototypically painted and lettered
- 2 mfx/DCC digital decoder with numerous sound and light functions
- 3 All four wheel sets driven by two powerful Bühler motors
- 4 Motorized pantographs, digitally controllable
- 5 Length over buffers 22-7/16 inches





Still-life with viaduct, 4/4 II and commuter train: From 1984 onwards, the Ge 4/4 II locos of the RhB received the red paint scheme that remains standard to this day, some technical upgrades and they peered into the world through rectangular rather than round eyes. The photograph shows the Ge 4/4 617 "Ilanz" with its train crossing the Wiesen Viaduct between Wiesen and Filisur.

the Furka Pass, which rises to an altitude of around 6,500 feet, and through the then new Furka Summit Tunnel to Brig. The two FO locos were needed because sections of this route are equipped with rack rails and are at such high gradients that locos not equipped with a cog system cannot run on them for safety reasons. In Brig, the loco was loaded and brought to Bern on the Lötschberg railway line and onwards via Zürich to Landquart, where it was put back onto the rails of the RhB.

Another high point in the life of 617 was certainly January 12, 2018, when its new anniversary design was unveiled to an industry audience – to mark the LGB anniversary year – at a small ceremony at the main Rhaetian Railway workshops in Landquart. Together with its older siblings – the Crocodile, the G 4/5 as well as the very first RhB loco, the Rhaetia – the loco was admired and extensively photographed there, naturally in the company of the corresponding LGB models. Accordingly, No. 617 Ilanz, which really does not show signs of its 45 years of service to date, will operate for a year on the RhB tracks in Grisons, where it is sure to feature not only on scheduled trains, but also on the special trains that will undoubtedly be arranged this year to mark the LGB anniversary.

The Ge 4/4 II 617 "50 years of LGB" as a model

The model of the Ge 4/4 II is a familiar sight to garden railroad fans, given the numerous versions of this loco that have previously been released. These include the five colorful anniversary locos of the Rhaetian Railway, most recently the "100 Jahre Arosabahn" (100 years of the Arosa Railway), which

was introduced in 2015. Naturally, there will also be a model of the Ge 4/4 II anniversary loco bearing road number 617 – prototypically printed with the design of the anniversary loco.

Technically speaking, it is identical to the other new Ge 4/4 II, and therefore features the new flush-mounted windows, additional rear view mirrors as well as roof-mounted antennas. Equipped with two ball-bearing-mounted Bühler motors and an mfx/DCC decoder with numerous lighting and sound functions as well as motorized pantographs, this heavy-duty powerhouse can handle any load and any gradient. Used as a general-purpose loco by the RhB, the Ge 4/4 II hauls everything that runs on rails in the Canton of Grisons – from express trains and passenger trains to freight trains all the way to shuttle trains. Naturally, the same applies to the model: Matching express-train and freight cars are available from the LGB range, and give every LGB fan the freedom to assemble their choice of train to match this loco. However, LGB also provides matching cars for the many special trains that will operate with this loco during the 2018 anniversary year, such as the salon cars of the Alpine Classic Pullman express.

Regardless of the choice of car – this loco will be the highlight of many garden railroads, making it an absolute must for the keen LGB enthusiast. It will be available as item number 28443 from early summer 2018 as a one-time series for the anniversary year exclusively from specialist dealers, so we recommend reserving a model in advance from your dealer. Even if the prototype of this loco will only run for a year in Grisons, the model will recall the wonderful and unforgettable anniversary year 2018 on garden railroads for many years to come.

Made by Märklin: Dirk Hundsdörfer about the design of the anniversary loco



Color magician: Dirk Hundsdörfer heads up the artwork group, which is responsible for the design of all models produced by the Märklin brands.



The model in the image features two print variants: The continuous fine color graduation on the left in the image is a digital printing specialty; tampon printing is particularly good at things like the thin, sharp line of the coat of arms. The dimensions of the prototype loco are shown in the image below.

The idea for the anniversary loco project emerged at the start of 2017. The background was that the RhB had already produced several anniversary locos with specific design concepts. It began with the “100 Jahre Berninabahn” (100 years of the Bernina Railway) in 2010, followed by “100 Jahre Chur-Disentis” (100 years of Chur-Disentis), the anniversary loco “125 Jahre RhB” (125 years of RhB); the “100 Jahre Bever-Scuol” (100 years of Bever-Scuol) and the “100 Jahre Arosabahn” (100 years of the Arosa Railway). The product management department hit on the idea of doing the same thing with “50 years of LGB”. The difficulty was that the predecessor locos featured colors such as blue, green and brown, which can integrate very well into a mountain landscape. But LGB uses this striking red color. How do I combine that with mountains? At some stage, I came up with the idea of the alpenglow theme, which incorporates red and orange tones. This resulted in the background with the mountains of the Bernina Range with this “glowing” light ambiance that occurs at sunrise and sunset. We then added the “50 Jahre” (50 years) lettering, the LGB logo and the typical LGB stripes in white and green. Since the RhB anniversary locos are well-known in Switzerland, we want to use this design to raise the recognition value, thereby bringing the life-size prototypes and our models together in visual terms.

First the prototype, then the model

Naturally, the design concept was coordinated with the Rhaetian Railway and change requests concerning issues such as color emphasis or the parallel arrangement of the locos in the image were incorporated. The latter required a great deal of image processing because the only available image presented the locos in a more radial arrangement. Once the adhesive films had been applied to the sides of the RhB locos only, thus avoiding the need to consider curves, there were very few process-related requirements for the prototype – apart from one: The resolution of the template naturally had to be oriented towards the size of the prototype loco. The uncompressed Photoshop file is 1.5 gigabytes in size, which provides an excellent level of detail on the prototype loco measuring almost 42 feet. Reducing the size of the image for the model is then not a problem. The reverse procedure should be avoided if possible as it results in a loss of quality. The requirements for the model are slightly different because the design is printed directly onto it. We have been combining tampon printing and digital printing for some time now. Tampon printing offers excellent edge definition, and it can be used to great effect to print small fonts, while digital printing is wonderful for printing very fine color graduations. The anniversary loco is an absolute gem for every LGB layout.



CLUB SCHEDULE: EXHIBITIONS IN SPRING 2018

Sinsheim and Dortmund



After the International Toy Fair in Nuremberg (January 31 to February 4, 2018), many of the new 2018 LGB models will be on show at Faszination Modellbahn in Sinsheim, Germany, from March 9 to 11, 2018 (www.faszination-modellbahn.com). Messe Sinsheim allows Club members a reduced entrance fee on presentation of their Club card. The Club team will then welcome Club members to Intermodellbau Dortmund (April 19 to 22, 2018) with a little memento of the exhibition. In the exhibition's online shop (www.intermodellbau.de), Club members can once again avail of discounted Print@Home entry tickets (10 euros instead of 12 euros). In the online shop, enter the code IB2018Maerklin – please use upper and lower case when entering the code.



The museum cars 2018

Jubilee car

Every year fans can look forward to new versions of the museum cars, which are only available at the Märklin Museum in Göppingen and traditionally operate with freight from medium-sized enterprises. So it basically goes without saying that the Museum has come up with a freight car of the Ernst Paul Lehmann Patentwerk, the birthplace of the G-gauge, to mark the 50th birthday of the LGB brand. LGB employee Hans Dieter Gall from Nuremberg (on right in image) is clearly over the moon with the new models. The private car of Ernst Paul Lehmann Patentwerk comes with the authentic paint scheme and lettering of era IV, opening and closing doors, and metal wheel sets.



Enthralled by the wonderful museum cars 2018 (from right to left): Hans Dieter Gall, Senior Production Manager at LGB, Märklin Managing Directors Florian Sieber and Wolfrad Bächle, and Wolfgang Hellstern, Managing Director of Confiserie Bosch GmbH.

Keep up-to-date with the latest company news and stories throughout the anniversary year

www.lgb.com/50years

This web page provides you with all the latest information about jubilee celebrations, special products or anniversary events, photos, movies, and everything you need to know about LGB's 50th anniversary. For instance, if you are planning to travel to Switzerland, you should definitely organize a ride on a train hauled by the amazing LGB anniversary locomotive Ge 4/4 II 617 "Ilanz" of the Rhaetian Railway (RhB). Discover the latest daily train numbers of the anniversary locomotive provided by the RhB loco service right here.

LAYOUT COMPETITION

We are looking for 2018's best garden railroad!



Whether it has complex structures or brilliant practical ideas, beautifully-planted garden layouts or thoughtfully-fitted-out indoor layouts, we are looking for your personal G-gauge dream come true.

Layout: Werner Säufferer, Baden-Württemberg, Germany, LGB Depesche 4/15

What have you got planned in the new season for your layout? Indoors it is worth paying attention to elaborate designs in confined spaces while outdoors the first warm rays of the sun and a breath of fresh air inspire you to develop new plans on a larger scale. And seeing as every railroad contains so much passion, enthusiasm, and energy it would be a crying shame not to share these amazing works of art with other garden railroaders and all the readers of LGB Depesche.

That's why the editorial team is looking for the best garden railroad 2018. Give us a surprise! Whether that means American landscapes with trestle bridges, Alpine mountain railroads with viaducts or simple ovals of track carefully set out in the existing landscape – we don't exclude anything.

The decisive factor will be the overall impression of the layout, and an expert jury will give its verdict. Join in – it's worth it!

IT'S SO EASY TO TAKE PART:

Questionnaire:

On www.lgb.de you will find a questionnaire on your layout we would like you to complete. If you do not have Internet access, write to the address stated below and request the questionnaire.

Photos: Please submit with your entry five detailed photographs, showing as much as possible of your layout.

Address: Please send photos and completed questionnaires by e-mail to LGB-Depesche@3g-media.de or by post to: LGB Depesche, c/o Gebr. Märklin & Cie. GmbH, Stuttgarter Strasse 55-57, 73033 Göppingen, Germany

Closing date for entries: July 31, 2018

Prizes: 1st price, DB Diesel Locomotive Class 212, (item 20120) or SOEG Diesel Locomotive Köf 6001 (item 23591); 2nd price, DR Passenger Car (item 36368 or 36354); 3rd price, RhB Low-sided Car (item 40092) or WP&Y RR Tank Car (item 40808).

In touch with nature

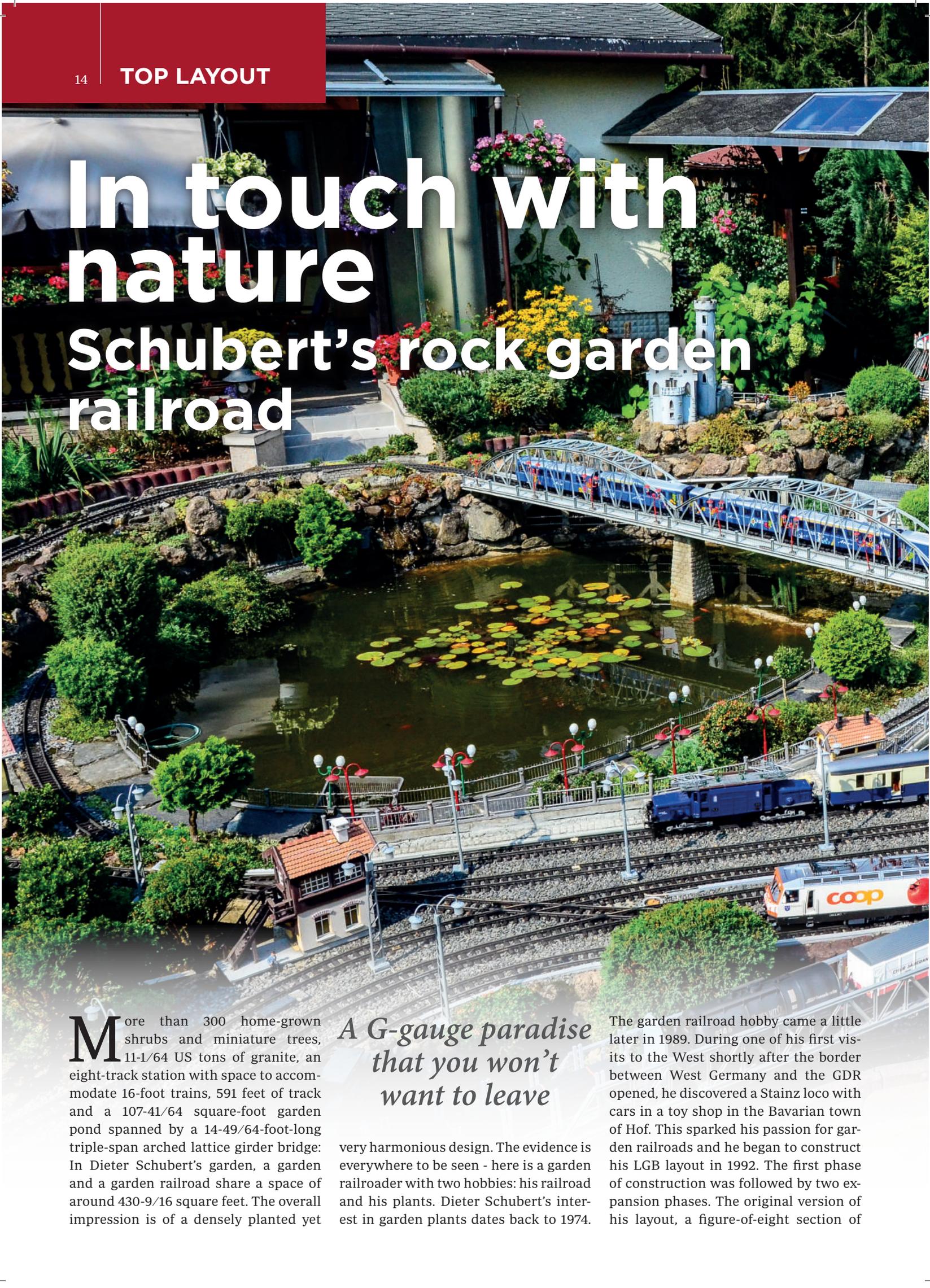
Schubert's rock garden railroad

More than 300 home-grown shrubs and miniature trees, 11-1/64 US tons of granite, an eight-track station with space to accommodate 16-foot trains, 591 feet of track and a 107-41/64 square-foot garden pond spanned by a 14-49/64-foot-long triple-span arched lattice girder bridge: In Dieter Schubert's garden, a garden and a garden railroad share a space of around 430-9/16 square feet. The overall impression is of a densely planted yet

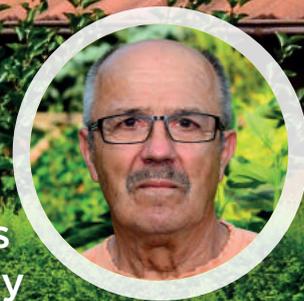
A G-gauge paradise that you won't want to leave

very harmonious design. The evidence is everywhere to be seen - here is a garden railroader with two hobbies: his railroad and his plants. Dieter Schubert's interest in garden plants dates back to 1974.

The garden railroad hobby came a little later in 1989. During one of his first visits to the West shortly after the border between West Germany and the GDR opened, he discovered a Stainz loco with cars in a toy shop in the Bavarian town of Hof. This sparked his passion for garden railroads and he began to construct his LGB layout in 1992. The first phase of construction was followed by two expansion phases. The original version of his layout, a figure-of-eight section of



Dieter Schubert has two hobbies: plants and garden railroads. Both complement each other perfectly on his 430-916-square-foot LGB layout and convinced the jury during the last LGB competition “Best Garden Railroad 2016”.



track around the pond, was expanded and extended to include two further loops with wider radii, additional tracks in the station area, tunnels, buildings, a waterside promenade as well as countless finely executed details.

He also added many plants, which give his layout such an intense and varied green appearance today. Dieter Schubert grew all of them himself and pruned them to integrate in the proper scale in his garden railroad landscape.

Box trees, cotoneasters, thuja smaragd (arbor vitae), cypresses and many others combine with the mighty granite from his surrounding area and various types of ground cover to create a varied rock garden ensemble that shimmers with numerous leaf shapes and shades of green. To ensure that the plants blend seamlessly with the landscape in 1:22.5 scale, Dieter Schubert lets his small trees grow for a few years, during which time he prunes them regularly, before placing

the miniature shrubs and bonsai trees into the numerous planting holes dotted throughout the layout between rock formations, walls and buildings.

One glance over Schubert's railroad reveals not only varied flora, but also numerous striking garden railroad highlights. The large station, the group of buildings in Christhausen on the nearby hill or the single-track arched lattice girder bridge are just some examples. The triple-span, 14-49/64-foot-long bridge, constructed ►



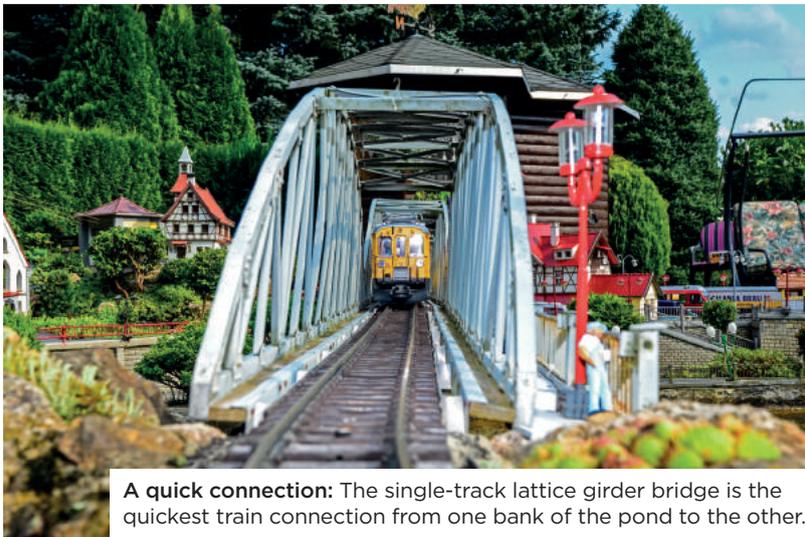
Castle tour: The castle not only has a moat, but is fittingly perched at the highest point in the garden railroad. In the background are the scratch-built carousel and the cable car.



Green landscape 1: The Bernina ABe 4/4 34 rail car make its way uphill to the bridge approach



Green landscape 2: The Bernina passes through the densely planted area around the old water mill.



A quick connection: The single-track lattice girder bridge is the quickest train connection from one bank of the pond to the other.



Valley station: A view over the houses of Christhausen and the valley station of the cable car.

entirely by Dieter Schubert crosses the 35-7/16-inch-deep pond situated at the center of the layout. In the background of the track layout, the elegant castle overlooks the scenery from its perch on the granite cliffs. It is the highest point of the garden railroad.

A stream, a waterfall and five tunnels

The row of green hills to the left of the pond also catches the eye. With its numerous granite cliffs, shrubs, bonsai

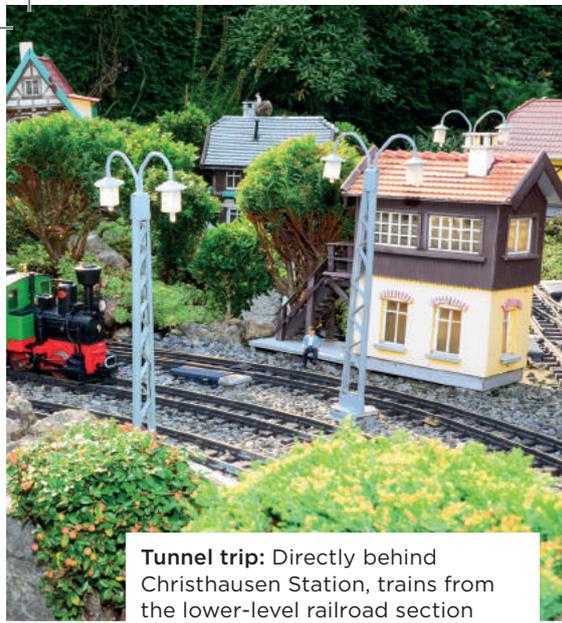
A rock garden clad in magnificent shades of green

trees and ground cover, it displays the flair of a carefully planted rock garden.

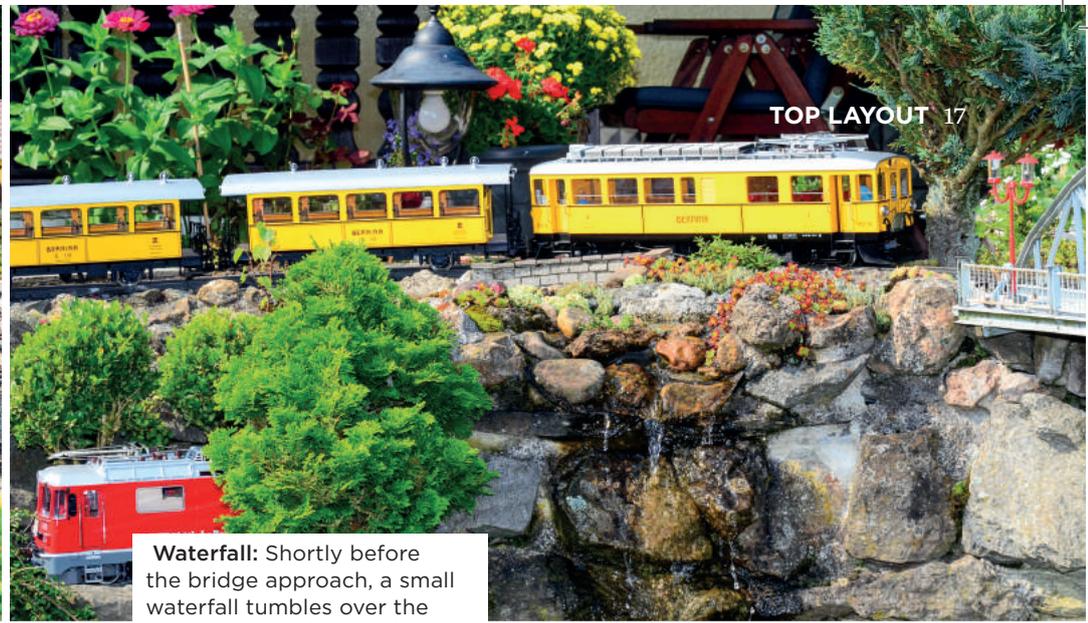
A closer look at the layout reveals further details. There is a walled 32-foot-long stream, which meanders from the mountain lake near the castle along the tracks

down to the water mill. Or the waterfall, which allows fresh water to tumble over granite cliffs into the pond just before the approach to the lattice girder bridge. Upon closer examination, one can see several tunnel portals, which indicate that parts of the layout are hidden from view. There are tunnels in the area around Christhausen Station, below the small town, but also on the other side of the pond, beneath the approach to the bridge. In total, there are five of them on the layout.

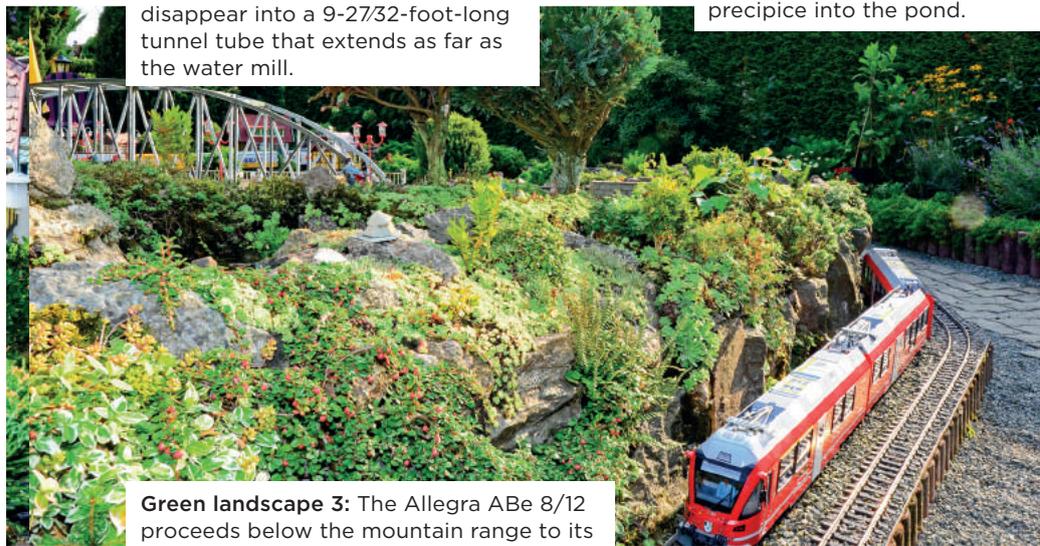
The beautifully designed promenade



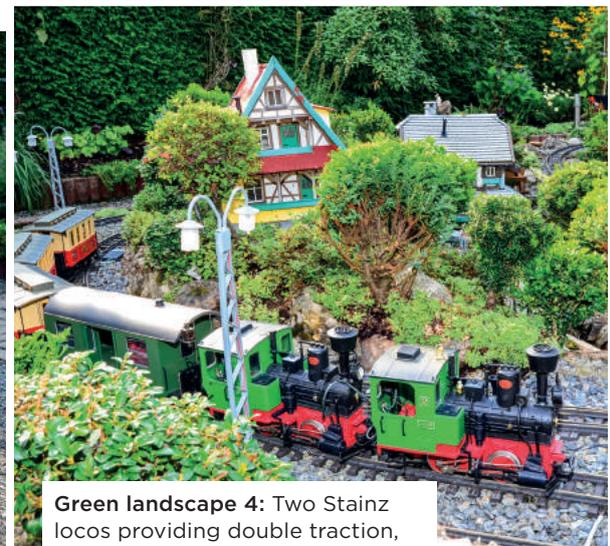
Tunnel trip: Directly behind Christhausen Station, trains from the lower-level railroad section disappear into a 9-27/32-foot-long tunnel tube that extends as far as the water mill.



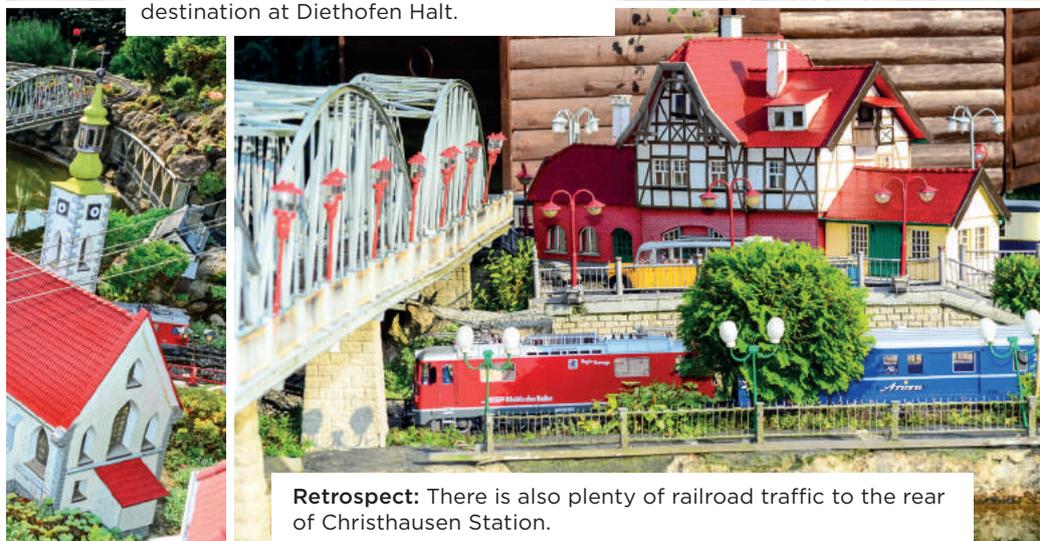
Waterfall: Shortly before the bridge approach, a small waterfall tumbles over the precipice into the pond.



Green landscape 3: The Allegra ABe 8/12 proceeds below the mountain range to its destination at Diethofen Halt.



Green landscape 4: Two Stainz locos providing double traction, pull their cars past the signal tower at Christhausen Station.



Retrospect: There is also plenty of railroad traffic to the rear of Christhausen Station.



Away from the tracks: The beautiful waterside promenade is the perfect place for a relaxing stroll.

on the bank of the pond is a highlight that only becomes apparent on closer inspection. The waterside walkway, which is lined with railings and street lamps, covers a large part of the pond bank. At other points on the bank, granite cliffs drop down to meet the water. Elsewhere, a protective gallery runs along the bank. The gallery, which opens towards the pond, protects the trains running along the lakeside from rockfalls and from snow in winter. Everywhere you look on Dieter Schubert's layout, there is some-

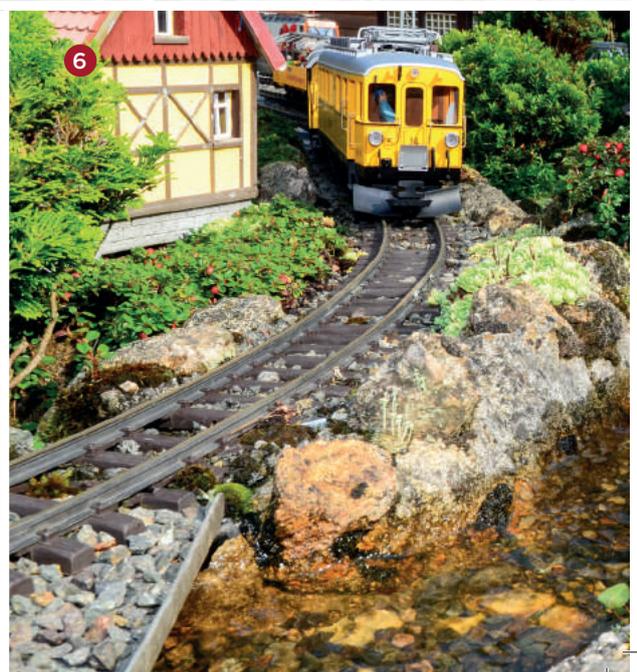
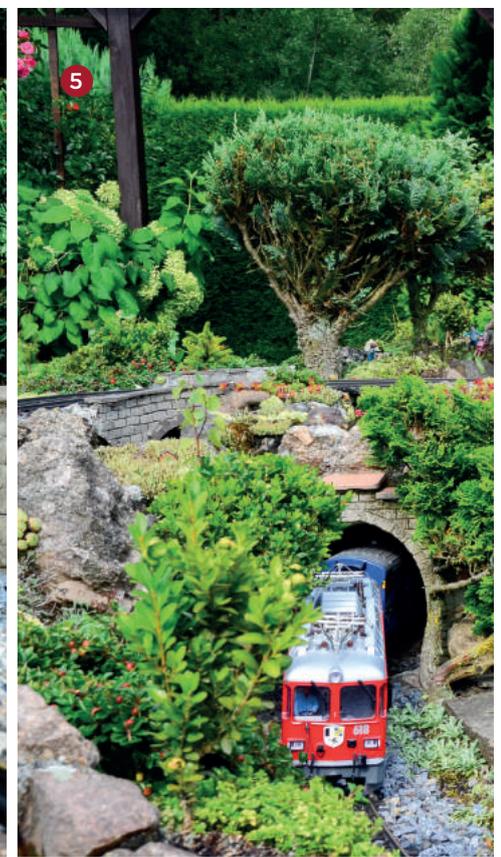
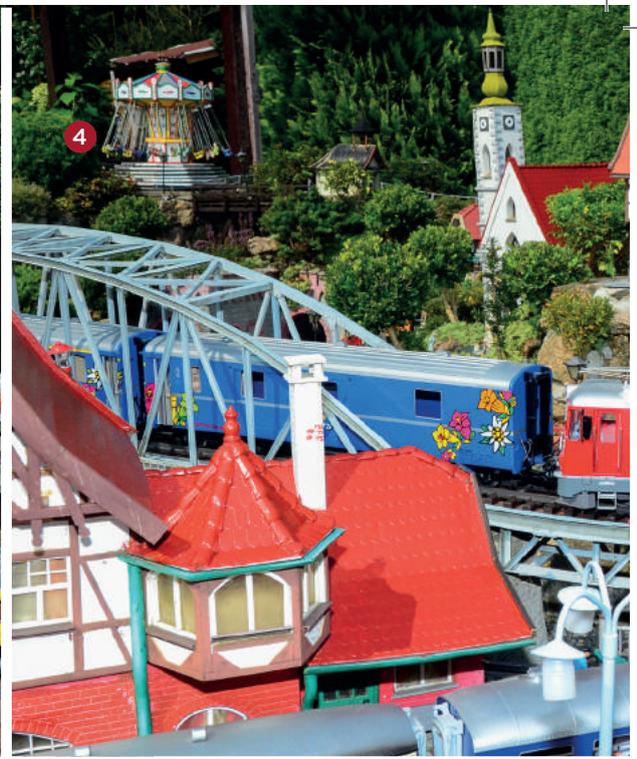
thing to discover. However, things start to get really interesting once the stock starts to roll. Up to four trains can operate simultaneously on the layout, which uses analog control.

The starting point for railroad traffic is the station at Christhausen, which takes up the entire front section of the layout. A total of eight tracks run in front of the timber-frame station building that Dieter Schubert constructed himself. All loops of track on the layout meet here – there is even sufficient space to accom-

modate long trains such as the Rhaetian Crocodile with five salon cars and a baggage car. As well as a through track, each route also has a passing siding. With no less than 14 turnouts, three crossings and a double slip switch, the station area provides numerous switching options.

A pond tour offers plenty of variety

The most varied route on the railroad is the one around the pond. It begins on one of the two tracks directly in front of ►



LAYOUT IMPRESSIONS

There is a great deal to discover on Dieter Schubert's 430-916-square-foot layout. The extensive planting, the varied landscape design and the complex railroad layout will impress both plant lovers and LGB fans.

1 A large station: Christhausen Station is the central station on the layout. In front of the lengthy building are six tracks at station level and two tracks in the lower-level railroad area. The tracks at station height are spread across the three track loops, each of which has access to a through track and a passing siding. The two tracks in the lower-level railroad area are passing sidings for the inner and middle track and connect these to one another. The tracks of the first and middle track loop in the station area can accommodate trains measuring over 16 feet long.

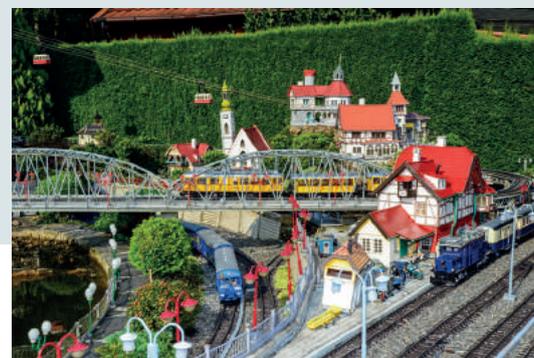
2 A hidden gem: The little halt at Diethofen is tucked away between bonsai trees and hedges at the outer edge of the layout. Trains on the middle and outer track loops pass the halting point at which only a few travelers wait for their connecting train. As with all the other structures on the layout, Dieter Schubert designed and built the little railway building himself.

3 Crossing the pond: The bridge over the pond consists of three arched lattice elements of equal length and is a total of 14-49/64 feet in length. Dieter Schubert built them from aluminum U-profiles. The bridge is supported by two concrete pillars measuring 4-17/64 feet high that stand in the pond itself, holding the bridge roughly 15-3/4 inches above the surface of the water. The bridge and pillars have been naturally weathered. Ten lamps that Schubert also constructed himself and mounted along the bridge provide suitable mood lighting at night.

4 Round trip: Behind the arched lattice girder bridge, the tracks connect with a 13-18-foot-long curved bridge approach, which arcs around the town of Christhausen in a 180-degree curve. Dieter Schubert built the approach as a gently banked curve to ensure that all trains could run on this section without difficulty. The aluminum bridge sections are supported by five concrete pillars, into the outer sides of which Schubert carved stone patterns in painstaking detail while the concrete was drying.

5 Tunnel tour: The tour on the inner track loop leads through three tunnels. The underpass at the approach to the bridge is a comparatively short tunnel section. It is preceded by a protective gallery that is open along the side facing the pond. Bushes, mosses and granite boulders are clustered around the tunnel, over which a small waterfall tumbles into the garden pond.

6 The stream: From the little mountain lake, a small stream flows along a walled river bed measuring over 32 feet in length down to the pond below the water mill. It babbles past the castle as it runs downhill along the ridge of the mountain. Beside the track on the inner circuit, it runs into the small water mill. Shortly after that, it disappears into a channel that leads into the pond. From here, the water is filtered and pumped back up to the little mountain lake.





Have fun: The colorful chain carousel decorated with over 400 LEDs was also built by Dieter Schubert.



Shining brightly: Schubert's easily removable "patented" lamps with clever plug-in bases provide light for the lower-level track.

the station building. Shortly after that, the route continues along a single track. On a clockwise tour, trains must negotiate a four percent uphill section behind the station area that leads up to the bridge. After crossing the pond via the triple-span arched bridge measuring a total of 14-49/64 feet in length, the train circles the little town of Christhausen on a bridge approach of about 13 feet, which rests on five supports. At this point, the train enters the tunnel underneath the hunting lodge on its way to the track section that runs alongside the pond. This first sends the train into a protective gallery and then into the small tunnel under the bridge approach. After that, the track continues around the pond along the waterside promenade. After passing behind the station at Christhausen and crossing the arched bridge, the train disappears into another tunnel. The underpass runs beneath the houses of Christhausen. After the train emerges from this tunnel again, it enters Christhausen station shortly after. Here, it can also avail of a through track as well as a passing siding. A double slip switch also allows it to switch to the central line.

The outer route to Diethofen Halt

This loop of track – like the outer line that runs parallel to it – leads around the entire layout and connects Christhausen Station to Diethofen Halt. The little railroad building nestles beneath the mountain range with its hilltop chapel, surrounded by ground cover, miniature bonsai trees and large hedges. Only a few travelers wait here for their connecting train. The track layout on the two outer loops is almost level. The tracks skirt the mountain range in wide radii and with virtually no uphill or downhill gradients. For viewers standing in front of Christhausen Station, the trains on this route are only briefly visible. A short distance behind Christhausen Station, they disappear behind a mountain range and only appear again after they have almost circled the entire layout and enter the tracks at Christhausen Station.

The lower-level railroad area at Christhausen

The trains that enter the two tracks situated at the very front of Christ-

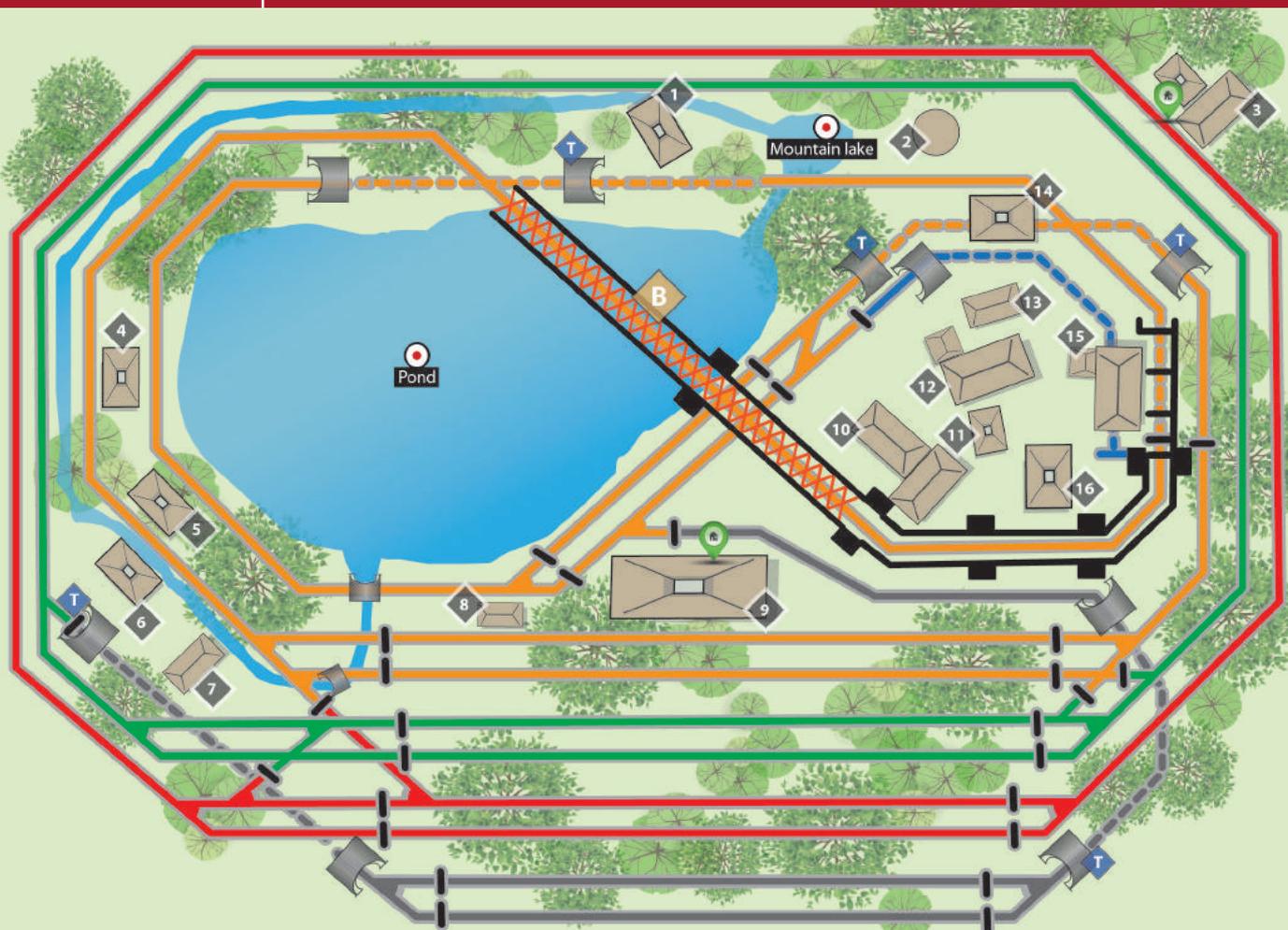
hausen Station are scarcely any more visible. This is the lower-level railroad area of the station. It runs below the other tracks. Not much of these lower-level tracks, which comprise a through track and a passing siding in the station area, is visible to the spectator. The two tracks coalesce in front of and behind the station to become a single track that soon disappears behind tunnel portals. A train that departs the station on this track in the direction of Christhausen and then enters the tunnel appears a few moments later beneath the arched lattice girder bridge and then reaches the inner loop of track.

The track route when a train leaves the lower-level railroad areas in the opposite direction is equally surprising. After proceeding through a tunnel section that measures nearly 9-27/32 feet, it emerges in daylight again at the same level as the water mill. There, it suddenly arrives at the second track loop, which takes it on a detour all the way around the layout as far as Christhausen Station.

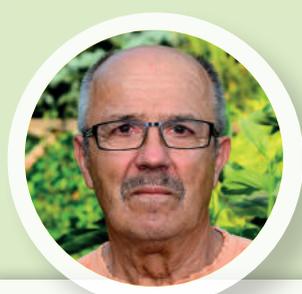
The station at Christhausen is one of the total of 17 buildings that Dieter Schubert designed and built himself. The castle, the hunting lodge, the restaurant "Zur Seilbahn", the church and the hilltop chapel, the two water mills, the cable car or the stopping point at Diethofen are other impressive structures created in his workshop. The scratch-built chain carousel with its 400 or so LEDs is another impressive highlight. In the evening, it really comes to life – as do the approximately 70 street lamps that Dieter Schubert designed himself and has distributed around his layout. He constructed different types of lamps, fitted with one, two or three LEDs, with varying mast shapes and in different colors. The lamps made of copper and brass are easily removable and can be refitted just as easily. Thanks to multiple split brass contacts in their shoe rail, they have a reliable electrical contact at all times.

Taking into account the level of detail on display here, it is clearly apparent that Dieter Schubert, a qualified electrician now retired, is not only a fanatic cultivator of plants, but also a passionate handicraft enthusiast and an experienced garden railroad inventor. Even after 26 years of garden railroading, he is always coming up with new ideas of what he could add to his layout.

TRACK PLAN | Layout description



1 Castle	4 Gatekeeper	7 Water mill	10 Restaurant "Zur Seilbahn"	13 Vicarage	16 Cable car	— Separation points
2 Chain carousel	5 Gatekeeper's lodge	8 WC	11 Kiosk	14 Hilltop chapel	T Tunnel	— Analog track loops
3 Halt	6 Residential building	9 Station	12 Church	15 Hunting lodge	B Bridge	

**Personal data: Dieter Schubert**

The qualified electrician Dieter Schubert (79) has created the perfect marriage of two hobbies in his LGB layout: his garden railroad and his garden plants. He cultivated the plants, and constructed most parts of the layout himself. From the approach sections and bridges, to the many buildings and a chain carousel all the railings and street lamps. He first came into contact with LGB in 1989. Although he sees himself as more of a garden railroad builder than an operator, he is happy to put trains on the tracks whenever visitors stop by.

Track length:	590 feet
Layout area:	430.5 sq ft
Running operations:	Analog
Turnouts:	16 turnouts, 3 crossings, 1 double slip switch
Setting turnouts:	Analog
Bridges:	2
Longest bridge:	Triple-span arched lattice bridge measuring 14-4964 feet long
Tunnels:	5
Longest tunnel:	9 ft 10 in
Stations:	Christhausen Station, Diethofen Halt
Houses:	17, all self-built
Pond:	13-184 feet x 8-1364 feet
Plants:	More than 300, all home-grown
Special features:	Tightly packed 430-916-square-foot layout with a very harmonious combination of railroads and plants. A varied track layout featuring a pond, a long bridge, a stream, home-grown bushes and miniature trees, scratch-built buildings and lamps.



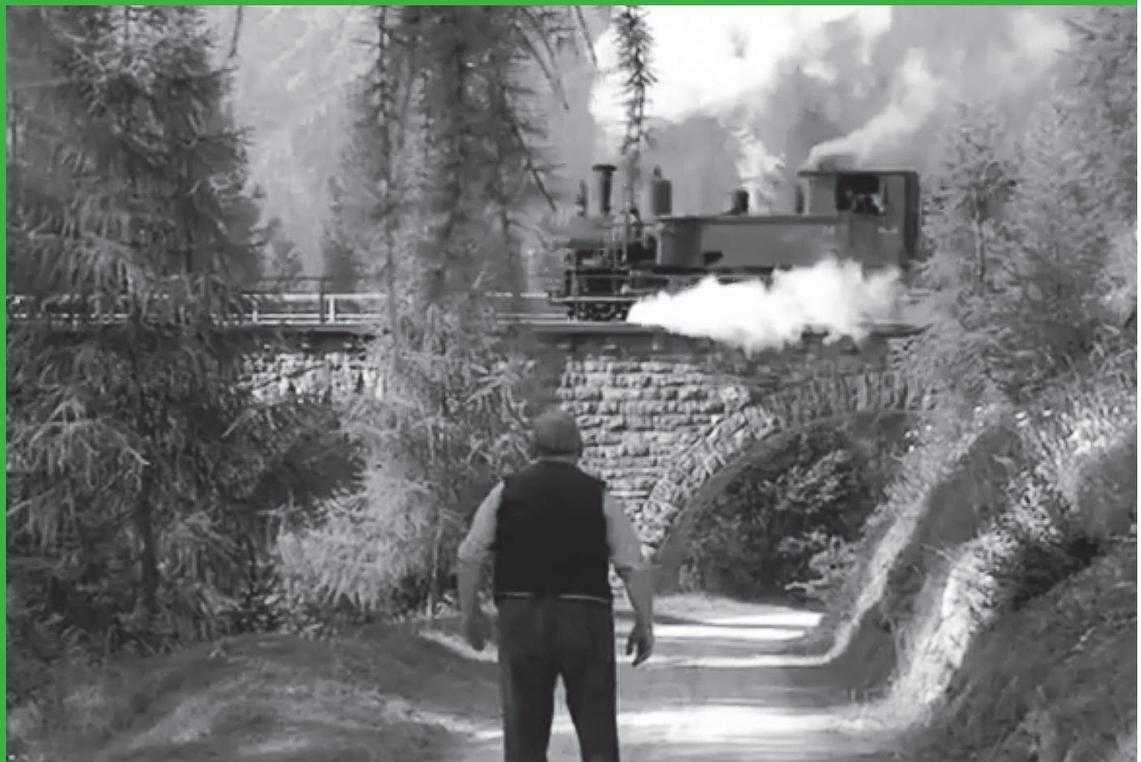
The G 3/4 11 steam locomotive has worn the name “Heidi” with pride since 1952, when it made a cameo appearance in the magnificent production of the eponymous movie. Along with Elsbeth Sigmund in the leading role and Heinrich Gretler as Uncle Alp, it played a supporting role – as did Theo Lingen. One might assume that this name was actually only assigned because the series 3 locos bearing the numbers 9-16 differed from their predecessors in that they did not have designated geographical names and therefore had to operate more or less anonymously. Be that as it may, the star of this children’s movie still brings spectators to tears today – tears of joy, that is – and not because “Heidi” is leaving her home, but because a 115-year-old lady can still appear so



A home among model railroaders: From 1977, the G 3/4 11 was in service with the Modelleisenbahnfreunden Eiger in Zweilütschinen, a district of Gündlischwand in the Canton of Bern. It was retired in 1990, purchased in 2000 by Club 1889 and modernized and restored to full working order by 2015.

Uncle Alp's darling

Almost 50 years after it was commissioned in January 1903, G 3/4 number 11 played the movie role from which its nickname is derived. Since 2015, "Heidi" has once again been puffing its way through familiar surroundings: the mountains of Grisons. A happy ending worthy of any movie.



Sadly, there has never been an Oscar for a steam locomotive: One of the most dramatic scenes of the 1952 movie "Heidi" takes place about 30 minutes in. Uncle Alp (Heinrich Gretler in the photo) can merely look on helplessly as a train with the G 3/4 11 at its head whisks his granddaughter away from her mountain home.

sprightly in her little black outfit and because she remains in rude health despite a lifelong smoking habit. We will ignore the boiler transplant and facelift for the time being.

Delivered in 1902, in service from 1903

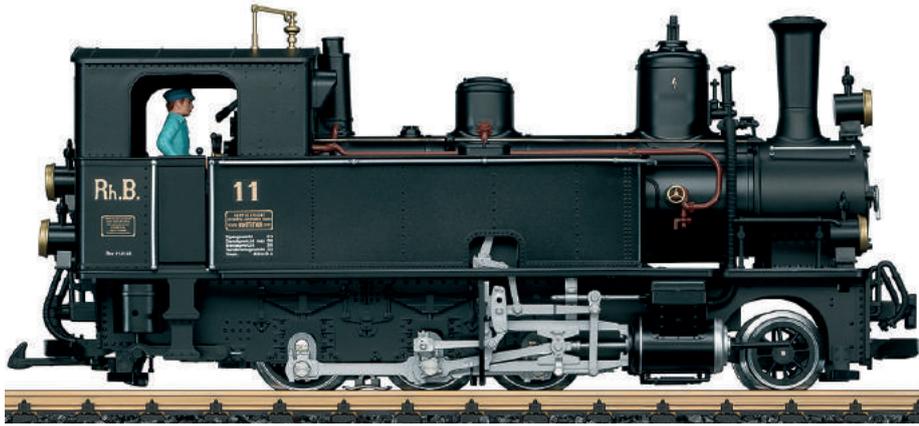
When it entered service on January 8, 1903 with the Rhaetian Railway to support its sister locos from the Schweizerische Lokomotiv- und Maschinenfabrik (Swiss Locomotive and Machine Works, SLM, Winterthur), it was already the fourth technical development stage of this Mogul class steam loco and featured the wheel arrangement 1'C - easily recognizable due to a weight increase of 4.4 US tons compared with the first series. The G 3/4 locos were used primarily

for light passenger and freight traffic as well as for construction trains. After the Rhaetian Railway completed the program of electrification of its network that ran from 1913 to 1922, the steam locomotives became increasingly expend-

The movie star among the steam locomotives of the Rhaetian Railway

able. Many of the locos in the fleet were disposed of as early as 1917, with some of them scattered to the four corners of the globe. Number 6 ended up in Brazil,

where it is now assumed to have vanished. Numbers 15, 16 and, subsequently, 9 and 10 were sold to the Brünig railway, which was, until 2004, the only narrow-gauge line of the Swiss Federal Railways (SBB) and operated the route from Lucerne to Interlaken. Locomotives 11, 13 and 14 remained with the RhB, where they performed switching duties or served as a steam-powered backup loco for use during power outages (see image at bottom left on the next page but one). Along with number 1 "Rhaetia", which was retrieved by the RhB in 1988 from the Blonay-Chamby museum railroad, and number 14 "Madlaina" of the "Dampflok-Verein Appenzeller Bahnen", "Heidi" is the only known surviving example of the G 3/4 locos today. As is typical of narrow-gauge railways, ►



A museum loco brand-new: Those who get the chance to see the G 3/4 11 "Heidi" today will immediately succumb to the excitement typical of steam aficionados. That is because this loco is an absolute gem. This model will elicit a similar response from garden railroad fans.



its fate followed a rather meandering path and this is perhaps the right moment to pay tribute to the six Swiss clubs and one foundation that operate under the umbrella association "Historic RhB" and whose work is dedicated to preserving the historic legacy of the RhB. One of these is Club 1889. Up to 2015, the energies of its Samedan project group in particular were focused on restoring and modernizing "Heidi". The club had purchased the loco in 2000 from the "Modelleisenbahnfreunde Eiger, Zweilüttschinen" (MEFEZ), where it ran since 1977 and was retired in 1990. The loco was initially transferred to Filisur and in 2004 to Samedan, where work to return it to full running order and to modernize continued until 2015. Perhaps the most striking detail of the modernization is that "Heidi" was converted to oil-fired operation. This meant that the steam loco no longer needed to be followed by a fire-fighting train due to the risk of fires caused by flying sparks. As a result, this testament to Swiss railroad history has been kept alive and up-to-date to this day.

The new "Heidi" by LGB for the anniversary year

Steam locomotive "Heidi" of the RhB

Era VI, item 20274

- 1 Authentic paint scheme and lettering of era VI
- 2 mfx/DCC-digital decoder with numerous sound and light functions, switchable in digital operation
- 3 Powerful, bearing-mounted Bühler motor
- 4 The model has many separately applied details, and the engineer's cab doors open
- 5 Smoke chuffing sounds are synchronized with the wheels
- 6 Length over buffers: 13-2532 inches

LGB is issuing a prototypical model of "Heidi" featuring the paint scheme and lettering of era VI, as it looks in service as a museum loco. A potent bearing-mounted Bühler motor drives all wheel sets on this star of the silver screen. "Heidi" has been updated to reflect the changes made to the prototype. An mfx/DCC decoder with numerous light and sound functions handles the digitally-controllable Swiss changeover of lights or smoke chuffing sounds that are synchronized with the wheels. What about operating sounds in analog operation? But of course!



"Heidi" and the cars: When it comes to restoring historical rolling stock, Club 1889 has also always achieved great things and continues to do so. The photo here, for example, shows the oldest passenger car of the RhB, formerly the Landquart-Davos-Bahn (LD, 1889) in a paint scheme from the year 1911 (C 2012 RhB) attached directly behind "Heidi". Time invested by the club: 5,600 hours.

Six clubs and one foundation maintain the historic legacy of the RhB

The "Historic RhB" association has been in existence since 2003 as the umbrella organization for six clubs and one foundation, whose stated objective is "the preservation, restoration and maintenance as well as the exhibition and operation of historic cars of the Rhaetian Railway on its network". The clubs involved are: Club 1889 (club1889.ch), Verein pro Salonwagen (www.verein-pro-salonwagen.ch), Verein Dampffreunde der Rhätischen Bahn (www.dampfvereinrhb.ch), Verein Bahnmuseum Albula (www.bahnmuseum-albula.ch), Freunde der Schmalspurbahnen (www.schmalspur.ch), Interessengemeinschaft Zügen-Landwasser (www.igzl.ch) and Albula Bahn Club Bergün (www.albula-bahn-club.ch). President Willy Hochstrasser, who was appointed in 2003, formulated the aim of the work carried out by the umbrella organization on behalf of the associations as follows: "To ensure that each of these clubs can focus fully on its own projects without getting tied up with administrative work, Historic RhB handles all public representation duties."



Experience history at close quarters: "Heidi" is not the only loco on show. The club "Verein Dampffreunde der Rhätischen Bahn", which was founded in 1977, is also organizing steam excursions with G 4/5 locos 107 "Albula" and 108 "Engadina" in 2018.

Club 1889 and the wonderful transformation of the G 3/4 11 "Heidi"



A stark contrast: "Heidi" spent most of its years of service with the RhB performing switching duties or as a steam-powered backup loco in case of power outages. The photo on the left shows the loco fitted with steps at Chur train station in 1964 (photo by Gian Brüngger in Gian Brüngger, Wolfgang Finke, Tram-TV DVD book 908 "Rhätische Bahn, Die Triebfahrzeuge 1" (Rhaetian Railway, The Traction Vehicles)). No words can fully describe its impact 51 years later (right!).



For over 20 years, Club 1889 with its 540 members has been restoring historic cars of the Rhaetian Railway on a voluntary basis. Its work groups in Chur, Samedan and Poschiavo work successfully on various ambitious projects. In addition to the oldest car of the RhB dating from 1889 mentioned at the bottom of page xx, the restoration of the Bernina Crocodile of 1928 is worthy of particular mention. The "Heidi" project under the direction of Armin Brüngger took the Samedan group a total of 10 years, from 2005 to 2015, to complete. The result: A time expenditure of 11,600 hours and material costs amounting to 1,298,000 Swiss francs have created a loco worth 2,110,000 Swiss francs. Today, "Heidi" boasts a new state-of-the-art boiler that uses low-sulfur heating oil, does not create any sparks that could cause line-side fires, and is equipped with an electric pre-heating system.

"Heidi" profile

Ordered by RhB; delivered 1902, commissioned January 8, 1903; new boiler delivered 2006; constructed by SLM, Winterthur; boiler built by Dampflo- und Maschinenfabrik DLM AG, Winterthur, and HKB Ketelbouw, Venlo (NL); oil firing: "DLM/Jucker" system; fuel EL heating oil; works numbers: 1476 (loco), 2675 (new boiler). Overall wheelbase: 185-364 in
Length over buffers: 332-364 in
Maximum speed: 27-31/32 mph
Weight (empty): 30-5564 US tons
Weight of water in the boiler: 2-3/4 US tons
Weight of water reservoir: 3-27/64 US tons
Weight of oil reservoir: 1807-51/64 lb (250-31/32 US gal @ 1-57/64 lb)
Oil throughput: 400 l/hr.

Lehmann-Gross-Bahn celebrates

All anniversary new products for the International Toy Fair in Nuremberg 2018



Bound by tradition: For many years, this “factory train” stood outside the Nuremberg LGB plant in Saganer Strasse. LGB is launching the model set with three authentic freight cars in a prototypical color and paint scheme of the 1970s.

Lehmann-Gross-Bahn has reached the ripe old age of 50. As the inventor of the garden railroad, the company owes much of its success to prototypes of narrow-gauge railways of the world. As a result, the new products for this important anniversary year not only include current classics, but also feature historic model milestones. What would LGB be without the “Stainz” steam locomotive, without the Rhaetian Railway, without superlative US products such as the US Mogul-type steam locomotive and models de-

signed specially for children? Quite apart from the fact that Lehmann-Gross-Bahn would never have come into being without the entrepreneurial boldness of its founding fathers, and that it may have ceased to exist but for the technical and commercial expertise of its current owners. So, dear friends of LGB: We all have good reason to celebrate. Also with regard to this anniversary year, which recalls an eventful history and permits us to look forward to a really bright future for garden railroad models. Let us go back to the early days:

When innovations give us cause to reflect

In keeping with the year of LGB’s birth in era IV, this year’s museum car (as always, available exclusively in the museum in Göppingen) will be a two-axle boxcar privately-owned by Ernst Paul Lehmann Patentwerk Nuremberg (item 40018). The LGB “Stainz” loco (see page 28/29) struts its stuff as a gold-colored variant complete with anniversary logo (item 20216) to haul

Here’s to the next 50 years: The model of the bar car on the Matterhorn Gotthard Bahn invites you to join the festivities, and will delight all G-gauge fans who also love winter sports with its impressive graffiti.



celebrates 50 years of G-gauge



A train makes garden railroad history

the five two-axle freight cars, arranged here in order of the five preceding decades and printed with images of the most attractive models in LGB's history (items 40501 to 40505). The Richter train (items 20214 and 36214) is also gaining a new member: The new mail car (item 32191) is designed to match, and is printed with a beautifully detailed motif. The model features interior details, sliding doors that can be opened and illuminated lanterns at the end of the train. The LGB landmark from the former plant in Saganer Strasse in Nuremberg will

be released as a factory train bearing loco number 99 5606, one of just two surviving steam locos from the Vulcan-class type i loco constructed for the firm Lenz & Co (the other being the "Franzburg"). The three cars in the set (item 29050) feature the color and equipment corresponding to their condition in the 1970s, with the two boxcars prototypically equipped with

inboard brake shoes only. The collectors' series with Steiff teddy bears captured the hearts of LGB fans both young and old, especially during the 1990s. To mark our 50th anniversary, customers can look forward once more to one of the original Steiff bears made from soft and cuddly plush material in the gondola (item 42229). The bear also wears the LGB anniversary logo on its chest. A "Christmas Train Starter Set" (item 70305) brings the anniversary year to a fitting close under the Christmas tree for all young entrants to the hobby. ▶



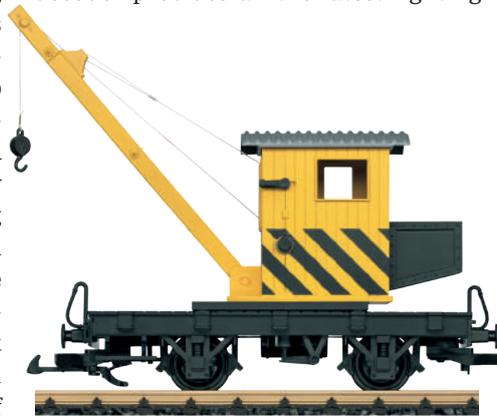


Splendid things from Switzerland

Who has not yet gazed in awe at the steaming, snow-blowing monster that is summoned into action for many major events on the Rhaetian Railway? The time has finally come! LGB is releasing the famous Xrot 9213 (item 26605). This 24-1/64-inch, finely detailed model is truly an awesome sight. Featuring heavy-duty metal construction, it is powered by two high-performance motors on both trucks. The doors to the cab and the inspection hatches for the blower drive mechanism can be opened. A separate motor powers the rotary plow wheel with its movable internal running gear. A manually adjustable ejection shaft and manually adjustable guide plates on the rotary plow wheel provide a realistic clearing effect. The paint scheme and lettering are prototypical for era VI, reflecting the appearance of the historic car that is still in use today. Equipped with an mfx/DCC decoder, the Xrot 9213 boasts numerous lighting and sound functions such as the Swiss changeover of headlights, firebox and cab lighting, operating sounds, a whistle that can be triggered by reed switches and much more besides. The built-in smoke generator also provides chuffing and cylinder steam sounds that are synchronized with the wheels.

Along with the magnificent Ge 4/4 II 617 "Ilanz" LGB anniversary loco, which is described in detail starting on page 6 of this issue, the G 4/5 106 is likely to be one

of the most hotly-anticipated locomotive models of 2018. The model version with numerous separately applied details features the paint scheme and lettering corresponding to the as-delivered condition of the prototype in era I. A ball-bearing-mounted Bühler motor delivers suitable propulsion for this 24-13/32-inch-long, hissing beauty, and an mfx/DCC decoder provides all the latest lighting



Hoisting, lowering, turning: This crane car does it all (item 40043).

and sound functions. A smoke generator with chuffing sounds synchronized with the wheels? But of course! The new class Tm 2/2 switching tractor of the Rhaetian Railway (item 21411) brings controllable couplings to the party and can uncouple attached cars at any point on the layout (in digital operation only). Six cars of the Rhaetian Railway are available for new train combinations and the bar car of the MGB (item 33661) with its dazzling graffiti is sure to attract attention in your garden.

Lehmann-Gross-Mogul

Did you know that LGB needed no less than 39 injection molds to produce a US Mogul-type tender loco (1'C) in 1985? With this loco, LGB set G-gauge standards in terms of detail at the time. As a result, these locomotives represent American pioneering spirit simultaneously in prototype and model form. This year's anniversary model also boasts numerous separately applied details and a ball-bearing-mounted Bühler motor that delivers power to all three wheel sets (item 20280). The anniversary logo is a point of honor. The emblem of the Napa Valley Wine Train adorns the F7 A-unit (item 20580) and matching cars (Observation Car, item 36591 and Boxcar, item 42915) of the eponymous train, which carries passengers who are wined and dined in gourmet style from the vicinity of San Francisco to the valley famed for its winemaking.

Last but not least

The German states will be represented in Nuremberg in 2018, initially by "Rasender Roland" with the RÜBB steam loco 99 4652 and two matching freight cars (item 40033 and 42270), as well as by additional cars from the German narrow-gauge scene. These also include the Saxon freight car 4333K (item 40271), which forms part of the world's most beautiful (I-k-) train. New items for summer and fall are sure to bring further surprises from around the world.

A US legend both as a prototype and model: The Mogul-type steam loco of the Denver & Rio Grande Western Railroad in its finely detailed anniversary look (item 20280).





50 years in a single train: Here, you can track the history of LGB presented as wonderful images of models. Naturally, the five cars and the anniversary "Stainz" loco will be available individually.



Four axles for 50 years: The model of an American streetcar is finished in opulent style. The doors open and the steps fold out (item 20383).



Will we meet this summer at Schönheide museum railroad? Saxony's oldest narrow-gauge railway offers open-air rides – on your layout as well (item 32352).



Long-awaited: The G 4/5 106 entered service with the RhB on April 30, 1906. The model replicates the condition of the prototype when delivered in era I (item 23530)

Strictly limited edition: The steam rotary snowplow Xrot 9213 of the RhB also fights its way forwards in model form with a heavy metal snowblowing mechanism (item 26605).



FURTHER NEW MODELS

In addition to the models shown in this issue, the following new items will be released at the International Toy Fair in Nuremberg in 2018:

LGB

- 20215 "Stainz" Christmas Locomotive
- 24681 Rail Truck
- 32191 Mail Car for the Richter Stainz Locomotive
- 36018 Christmas Car for 2018
- 70305 Christmas Train Starter Set

DR/RüBB/SOEG

- 24267 RüBB Steam Locomotive, Road Number 99 4652
- 40033 RüBB Gondola
- 40271 SOEG Saxon Freight Car, Car Number 4333K
- 41591 DR Stake Car
- 42270 RüBB Boxcar
- 46834 DR Tank Car
- 49180 DR Roller Car Set

RhB

- 21411 RhB Switching Tractor
- 31681 RhB Dining Car
- 33401 RhB Passenger Car, Car Number AB 22
- 33402 RhB Passenger Car, Car Number C 209
- 33403 RhB Baggage Car, Car Number F 4051
- 33669 RhB Panorama Car, 2nd Class
- 43814 RhB Boxcar

USA

- 20580 Napa Valley F7A Diesel Locomotive
- 36591 Napa Valley RR Observation Car
- 41915 Napa Valley Boxcar

Steiff is back on track: Have you ever encountered a bear when traveling by railroad? G-gauge makes it possible. With warm regards from Ernst Paul Lehmann Patentwerk.



New accessories for garden railroaders

An RhB station, a complete coal merchant, three heavy-duty juggernauts, a yellow track cleaner: New products for fantastic garden railroad scenes.

FALLER/POLA www.faller.de

Romantic RhB

The Rhaetian Railway is the number one theme on many LGB layouts - with good reason: The romance of what is perhaps the world's most famous and beautiful narrow-gauge railway is simply magical. Pola now presents another building to recreate that unique RhB atmosphere on your railroad at home. The train station "Susch" (item 330999) is a delightful structure finished in typical Engadine style and serves both as a reception building and a freight depot. This sturdy halting point that is clearly constructed to defy mother nature - the prototype is built to withstand harsh Swiss mountain conditions on the 'Engadine Line' of the Rhaetian Railway from Pontresina to Scuol - features characteristic elements such as the deep reveals at the window and door openings and a low double pitch roof with supporting wooden beams.

A rustic machinery shelter (item 331088) makes an excellent addition not just in an RhB setting, but also in other rural surroundings. This large, semi-open structure finished in a wooden appearance is suitable for storing machinery, cars and various tools and implements. The rear-facing window shutters can be moved, and the structure of beams and joists with its authentic breakthroughs gives it a lifelike appearance.



SUSCH STATION (ITEM 330999)

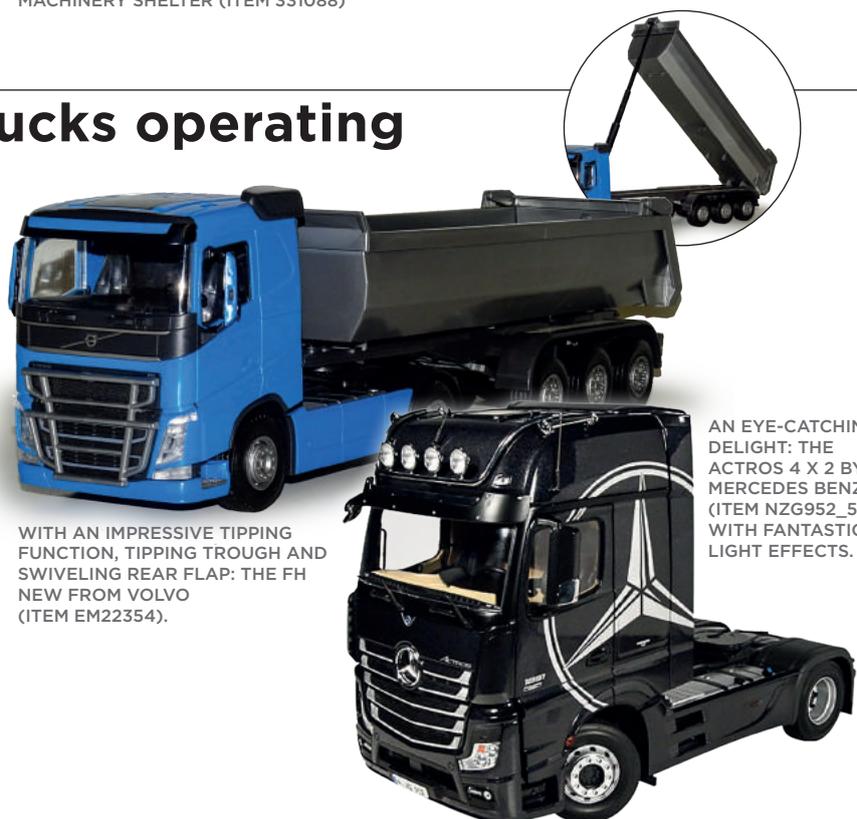


MACHINERY SHELTER (ITEM 331088)

EMEK www.lkwmodelle.de

Caution, heavy trucks operating

Vehicle specialist Emek is releasing three new items that are compatible with G-gauge. And this trio of heavy-duty juggernauts is extremely capable: To begin with, the Volvo FH NEW (item EM22354) with a tipping trough and swiveling rear flap is an imposing tractor unit that clearly displays its appetite for heavy loads. The Volvo FH GL XL 4x2 (item EM81334) can also pull its own weight. Both models in 1:25 scale are compatible with all other Emek trailers, fitted with rear-view mirrors, and absolutely weather-proof. However, the Mercedes Benz Actros 4x2 by NZG in 1:18 scale with a tiltable cabin, steerable front axle, opening doors and engine (item NZG952_50) is likely to remain the top dog for the time being. The illuminated cabin and Mercedes star make it an impressive highlight.



WITH AN IMPRESSIVE TIPPING FUNCTION, TIPPING TROUGH AND SWIVELING REAR FLAP: THE FH NEW FROM VOLVO (ITEM EM22354).

AN EYE-CATCHING DELIGHT: THE ACTROS 4 X 2 BY MERCEDES BENZ (ITEM NZG952_50) WITH FANTASTIC LIGHT EFFECTS.

PREHM www.prehm-miniaturen.com

Prehm digs coal

Right up to the late 1960s, coal merchants were an indispensable feature of every city and village. Therefore, the theme of coal should really be incorporated on every historical layout. Prehm is now releasing two exciting coal buildings as part of its range of low-relief structures. The large coal merchant (item 550750) is a finished model with a gate and platform. The plastic model is hand-painted with UV-resistant colors. The key feature: absolute weather-resistance. An ideal addition: A coal depot (item 5507519) that also stores other types of fuel such as wood, briquettes or turf as well as a coal scales (item 500550) to weigh the sacks. And who runs the business? Strapping lads such as the worker who is filling a coal sack (item 500702) or a colleague dressed in a thick apron (item 500701). Completing the team is the company boss (item 500700). All figures are made of metal. And just in case of accidents, a team of paramedics will be at the scene in no time at all thanks to the fantastic VW Crafter. More about this ambulance set in the next issue.



COAL MERCHANT (ITEM 550750)



COAL SCALES (ITEM 500550)

COAL
WORKER WITH
COAL SACK
(ITEM 500702)VW CRAFTER
(ITEM 500300)
PLUS MEDI-SET
(ITEM 500133)GROOTSPOOR www.grootspoor.com

Blow hard please

Do you want clean tracks and dust-free sections of rail? Then make way for the fan car by Grootspoor. This cleaning car made of robust plastic suitable for outdoor use is equipped with two turboprop motors which generate a powerful air stream to clear the tracks of dust particles and small objects. The car is controlled by a Zimo DCC decoder that draws current from the track; flashing lights can be activated. By the way: The Dutch accessory experts are giving readers of LGB Depesche a free oil pen worth 9.95 euros on all orders of 50 euros and above. This silicone-free, high-tech oil is heat-resistant up to 260° degrees and is suitable for all types of metal, plastic and acrylic. The thin hollow dispensing needle also allows oil to be applied at hard-to-reach spots. Simply enter the discount code "Depesche Ölstift" when submitting an order. This offer is valid until March 15, 2018 and may be used once per customer.

FAN CAR FOR CLEANING
TRACKS (ITEM FANCAR)



Stefan Kühnlein, from Hummeltal in Franconia, has already completed the biggest leap between gauges. He started with a Z-gauge layout, and then came the conversion to LGB.

STEFAN KÜHNLEIN

Enjoyment shared is twice the fun

We are in the year 2018. Endless stories are being told about garden railroads. The 50 years of LGB, the model railroad for indoors and outdoors, also seem endless. My congratulation on this “golden” anniversary. Immediately after that, the company magazine for fans of LEHMANN-GROSS-BAHN...from Nuremberg back then... will also celebrate 50 years in January 2019. Issue number 1 was published in January 1969 with the title “Die LGB hat Geburtstag” (LGB celebrates its birthday). Birthdays, anniversaries – these are all occasions that people celebrate together. The spirit of togetherness is also important in relation to model railroads. Togetherness implies that things or characteristics are in harmony. I first experienced this with the garden railroad I shared with my two children. Putting the tracks together and running trains on my own was certainly enjoyable. But it was the way the children’s eyes lit up that made playing together so much more fun. Oh yes, I should point out that this was in 1986. At that time in Germany, we already had “Bildschirmtext” - an interactive video-text service - and cell phones that used an analog network called the “C-Netz”. The sense of togetherness in relation to model railroads or their prototypes was largely associated with meeting, traveling and sharing information with others. Model railroad-ing was more fun as part of a community or club, which is a voluntary and long-term community of individuals who share the same interests and goals. Is this still the case today? I can honestly say that it is. Even in 2018, there will be meetings at all types of model railroad clubs where people will gather to run LGB trains together. Garden railroad retailers have also recognized that hosting events on their premises in turn stimulates many clubs and their members, and attracts them to the LGB hobby. Many people pool their ideas here and share them with those who attend

the events in order to learn more. One thing is for sure, EVERYONE has fun in the process. Join me on a journey back in time to 1996. The Rhaetian Railway, then led by travel manager Geni Rohner, announced a trip with the steam rotary snowplow Xrot. In February 1996, a group of “railroad nuts” from Germany, Austria, Switzerland and even the US gathered, bringing with them a great many shared interests: Railroads, snow, ice, frost, sun, steam, spectacular sights, traveling and, above all, membership of a model railroad club. At the time of the trip, there was not a great deal of snow in Grisons. The trips undertaken together on the RhB trains, the jaw-dropping sight of the huge red Xrot rotary plow wheel, the communal midday and evening meals and the games of curling are what made the hobby even more enjoyable. Standing alone in front of the Xrot, sitting alone on a train, or witnessing the other delights alone would have diminished the experience significantly. The friendships formed in this “group” based on the shared enjoyment of model railroads endure more than 20 years later. The year 1996 had something else in common with the LGB railroad. That July, the city of San Diego in southern California was not only a destination for my friend and I, but also the meeting point for the community of LGB fans. Railroad travel groups, members of garden railroad clubs and the management of LGB and LGB of America at the time spent a couple of days celebrating together. Club members from the US, Canada and Europe discussed garden railroad layouts and participated in tours of the plants with great joy and enthusiasm. I would like to thank everybody who made such a shared experience possible. I think that it is twice as much fun to pursue this hobby together with others in a club. Above all else, there are always people who are committed on a voluntary and ongoing basis to keeping the wheels of garden railroads rolling.



A trip on the Xrot: A good reason for LGB fans to experience railroads together.

50
Jahre | Years



1968-2018: LGB history part 1

PAGE 34: THE FIRST 25 YEARS
PAGE 38: DO YOU KNOW LGB?
PAGE 40: ANNIVERSARY TRIPS

**The railroad for
indoors and outdoors**

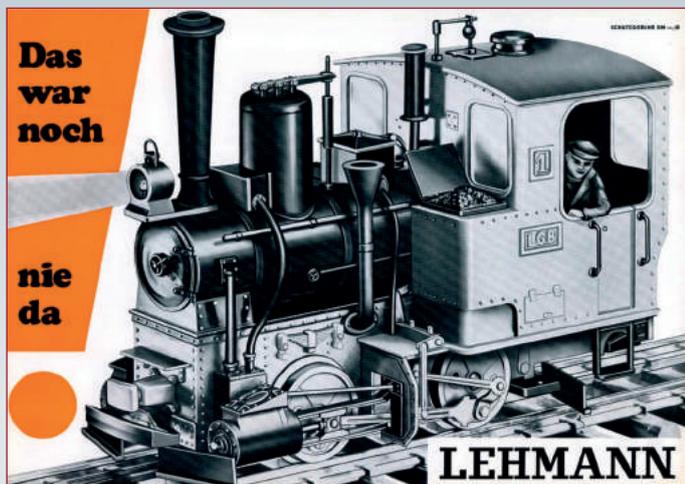
“This is new!”

A look back at the first 25 years of LGB

In February 1968, winter was in full swing as the International Toy Fair got under way in Nuremberg: Seeking shelter from the snow storm outside, all the visitors would have headed straight to the warm exhibition halls. Had it not been for the model railroad locomotive that grabbed everyone’s attention: There it was, in the middle of the snow, right in front of the exhibition center, boldly running around the expansive layout – completely unperturbed by the wind and weather. Innumerable passers-by and visitors stopped and looked on in astonishment at the spectacle: A model railroad, running outdoors? Impossible!

Even in its wildest dreams, the German company Ernst Paul Lehmann Patentwerk never expected the aura of success sur-

rounding the unveiling of the “Lehmann-Gross-Bahn” (or LGB as it became known): Numerous national and international retailers and journalists were enthralled. The new railroad was, for many of them, the highlight of the exhibition. Although this big success was anything but certain. It had taken three years to plan and prepare the model railroad. The two proprietors and founders of LGB, Eberhard and Wolfgang Richter, had already presented wooden samples of the models to a panel of experts at the Toy Fair back in 1965. And although their reactions were less than encouraging, the two brothers were not deterred and continued to develop their idea of a revolutionary model railroad. They realized that it not only needed to be big, but also had to be sturdy: It should be possible to operate it both indoors and outdoors.

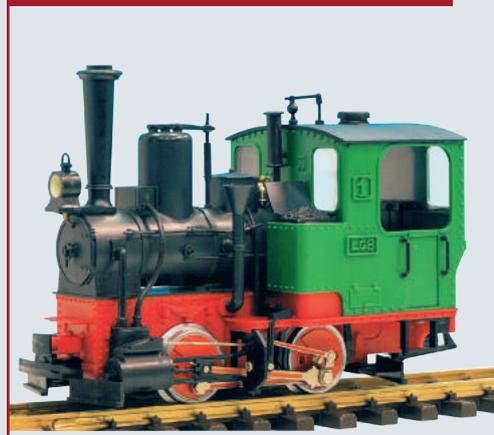


Revolutionary new product: The cover of the first ever LGB catalog in 1968 proudly proclaimed: “This is new!”

THE INITIAL LGB RANGE

- 2010 Tank loco no. 1 of the Salzkammergut Local Railroad
- 2020 Tank loco no. 2 of the Styrian State Railroads
- 2040 Tank loco no. 4 of Hüttenwerke Siegerland
- 3000 Passenger car of the Lower Austrian State Railroad
- 3010 Passenger car of the Salzkammergut Local Railroad
- 3020 Baggage car of the Pinzgau Local Railroad
- 4000 Flat car of the Salzkammergut Local Railroad
- 4010 Low-sided car of the Salzkammergut Local Railroad
- 4020 Gondola of the Härtsfeld Railroad
- 4030 Boxcar of the Salzkammergut Local Railroad
- 4031 Boxcar of the Ziller Valley Railroad
- 4040 Tank car of the Ziller Valley Railroad
- 20301 Passenger train starter set
- 20401 Freight train starter set

THE FIRST 25 YEARS OF LGB



1968

Starter model “Stainz”:
 The world premiere of the “Lehmann-Gross-Bahn” at the International Toy Fair in Nuremberg was a huge success.



1969

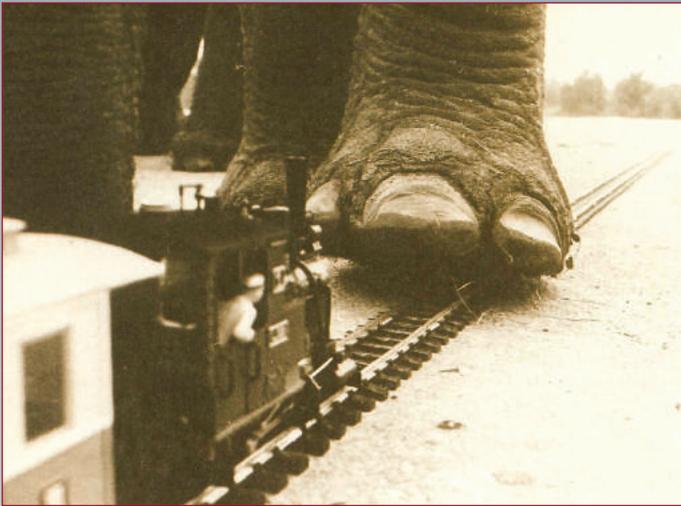
Congratulations!
 LGB celebrated its first birthday and LGB Depesche congratulated the “birthday child” in its first edition.



1974

The first large LGB steam loco:
 The Trans-Harz Railway steam loco (2080S) was one of the first models with realistic sound effects.

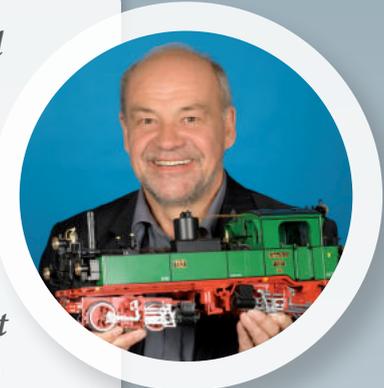
PHOTOS: LGB, LOK LGB BOOK (FARNETI/ZANINI/TURCI), JAMES SHOURT (WWW.SHOURTLINE.COM), LGB CLUB RHEIN-SIEG



As sturdy as they come: In 1970, the circus elephant Hassan was allowed to test the robustness of LGB rails.

Producing a railroad for the backyard was a fantastic idea by LGB. Even if my green IV k simply resides on a shelf in the living room. All the best for the next 50 years.

H. v. Ortloff



The new star of the model railroad world was therefore equipped with some special properties: Dust-tight enclosed motors and gear drives ensured the railroad was resistant to carpet fluff indoors and to moisture, temperature fluctuations and UV light outdoors. This was also enabled through the use of various plastics, a relatively new material at that time. The LGB was designed to be a “hands-on” model railroad and thus extremely robust and versatile. This also included, for example, opening and closing doors. Top quality in every aspect of their work was the order of the day. Brass, authentically profiled rails guaranteed safe power transmission, while being sturdy and weather-proof. And, above all, LGB was intended to be great fun: The ultimate model railroad experience that brings the whole family together.

The first LGB range contained three locos: The “1” (2010), based on a prototype of the Salzkammergut Local Railroad, the “2” (2020) featuring the now legendary “Stainz” of the ▶



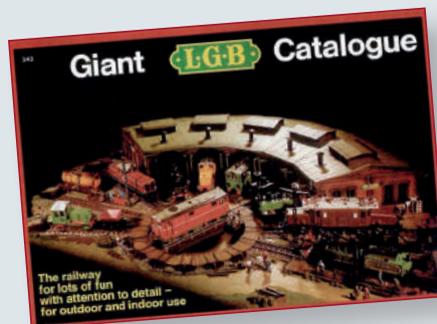
Two locos at the same time: LGB also launched a catenary wire in 1971 that enabled the simultaneous operation of two trains. For example, with the E1 of the Mixnitz - St. Erhard State Railroad (2030), one of the first electric locos.



1977

Robust, weather-proof, and great value for money:

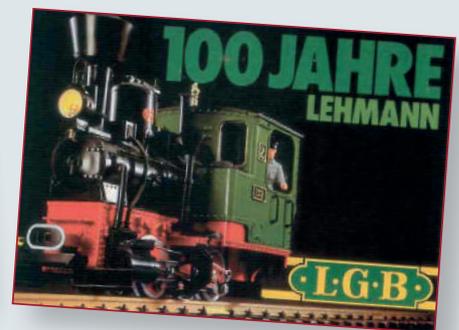
The Primus Super Railroad was sold by department stores and supermarkets as a simplified LGB version.



1978

First LGB club in the USA:

The number of fans of the model railroad for indoors and outdoors continued to rise, also overseas, and the LGB family welcomed new members.



1981

Big company anniversary:

Ernst Paul Lehmann Patentwerk celebrated the foundation of the company in Brandenburg an der Havel in 1881.



Successful anniversary train: LGB launched this special train in 1985 to mark the 150th anniversary of the first German railroad. It was limited to just 15,000 models.

Styrian State Railroads, and the “4” (2040), based on an industrial loco of the Hüttenwerke Siegerland.

Just one year later and the LGB range had already been extended: It now included a streetcar steam loco of the Upper Rhine Railroad Company (“Feuriger Elias” (Fiery Elias), 2050) and the diesel loco Schoema CFL 150 (2060H). The range grew to encompass eleven cars. At the same time, in January 1969, the first edition of LGB Depesche appeared, which subsequently became a quarterly magazine full of articles about LGB products and their prototypes. The pages were also filled with tips and tricks on how to use new materials and to construct weather-proof layouts.

Over the following years, LGB continued to develop its range to meet the ever increasing requirements of its customers. A smoke generator was first installed in the Stainz model as standard from 1970 onwards. In 1971, the new catenary wire enabled the use of two trains on one track. By then, the range already boasted nine locos and thirty-one cars. By 1979, the number of traction vehicles had risen to twenty-two. In terms of sound, LGB carried out pioneering work: In 1972, the Schoema CFL 150 was fitted with a powerful, battery-operated signal horn that was actuated via a contact strip. And in 1974,



Hand work: An LGB employee assembling Stainz models (2020) in 1975.



1985

The first LGB club in Germany: The “LGB-Club Rhein/Sieg” was founded in the town of Much. It has been followed by many more LGB fan clubs over the years.

1986

Steep incline: The first rack loco appeared based on the HGe 2/2 of the Furka-Oberalp Railroad in Switzerland.

1987

Big in the USA: LGB took its destiny in North America into its own hands by establishing the US subsidiary “LGB of America”.

LGB fans were amazed by the bell signal, the whistle sound, and the steam loco sound effects of the Trans-Harz Railway steam loco (2080S), the latter was even synchronized with the motion of the wheels. The first loco with diesel engine sound, the ÖBB model 2095.03 (2096S), followed in 1983. The era of digital sounds commenced in 1991.

LGB was becoming a well-known and much-loved brand, as the LGB family continued to expand. Between 1974 and 1986, starter sets with a reasonably priced yet robust battery-operated railroad appealed, in particular, to families with young children and ensured the number of LGB users rose steadily. Adverts featuring the biggest German stars of the day such as Hans Rosenthal and the appearance of LGB trains in TV shows enhanced this success even further. When Ernst Paul Lehmann Patentwerk celebrated its 100th anniversary in 1981, LGB held a roughly ten per cent share of the model railroad market. The 19,000 models of the Anniversary Express, which was produced to commemorate the occasion, were an overwhelming success.

And it was this anniversary model that kick-started a series of special models of locomotives and cars with special equipment and for limited edition train sets. After 1981, virtually every year witnessed at least one collector's model from LGB or one model commissioned by various clients, from breweries and railroad companies to municipalities. To mark the 150th anniversary of the railroad in 1985, LGB presented a highly colorful special train limited to just 15,000 models.

The increasingly detailed design of the rolling stock, the true-to-life loco sounds, extensive historical-technical descriptions, and photographs of the prototypes ensured that LGB gradually became accepted by even the most die-hard model railroaders. Limited, handcrafted, and highly detailed exclusive models by the model-maker Christian Höhne were produced in small series each year between 1977 and 1983. The full metal models are now highly sought-after by collectors.

LGB was also boosted by strong international growth throughout the 1980s. The Mogul tender loco (2018D), which LGB launched in 1984, was the first prototypical US loco and can be seen as a direct response to the sharp rise in the number of garden railroaders in the USA. It was followed by a large number of US-style locos and cars. In the meantime, sales to the USA accounted for more than half of all exports. From

The fan club "LGB Freunde Rhein/Sieg" has been loyal to the LGB brand for the past 32 years. We would like to congratulate LGB on its 50th anniversary and will remain closely associated with the brand in the future.

Günter Schippers,
Chairman of the fan club LGB Freunde Rhein-Sieg e.V.



1987 onwards, the company's US trading subsidiary "LGB of America" was responsible for booming sales in the USA. The first edition of "LGB Telegram", an English-language version of LGB Depesche, was launched in 1989 and the magazine was produced fully in the USA from 1991 onwards. US customers appreciated the intensive support and service that LGB offered and this helped to drive sales even further.

Meanwhile, back in Germany, the fall of the Berlin Wall led to turbulent times. Discover what German reunification meant for LGB and which railroad models LGB launched in the following years in the next issue of LGB Depesche.



LGB catalog 1993: LGB took the opportunity of the 25th railroad anniversary to present itself in a shiny gold look.



1989

Automatic pantograph:

The GE 4/4 II electric loco (2043) automatically lowered and raised the pantograph when changing the running direction.

1990

Large US diesel loco:

The White Pass & Yukon (20550) was the first LGB diesel loco with three-axle trucks.

1993

US steam loco Forney:

The popular steam loco with integral tender helped to increase the still modest number of US locos at LGB.

Everything you always wanted to know about LGB



As far as the eye can see

Since 2012 the title of the longest LGB model has been held by the Allegra rail car, an accurate representation of its Rhaetian Railroad prototype. The overall length of the model is 81 inches. Two fully furnished and illuminated engineer's cabs as well as fully furnished passenger compartments in all three vehicle units really whet the appetite for more. Additionally, there are opening and closing doors, spring-loaded buffers, and numerous richly detailed attachment parts at each front end. The two pantographs at the powered end cars can be raised and lowered digitally. The modern-day prototype has been running on the 239-mile-long railroad network in the canton of Grisons since 2009. "Allegra" is a customary greeting used in the Rhaeto-Romanic language and can be translated as "rejoice!"

A "howling" shame

It cannot be said that the first LGB models spread a tone of doom and gloom. Quite the opposite, they were hugely popular and successful. And, nonetheless, their motors were soon christened "howlers" by many users. This was because the gear drive of the motors created a typical screeching or howling noise. The instruction manual of the day called for a little patience and stated that the whistling would stop after roughly 200 operating hours. Subsequent models were soon equipped with a worm gear to prevent the "howling" noise under load.



One of the first howlers: The streetcar steam loco "Feuriger Elias" (Fiery Elias) (2050) was launched in 1969.

Once brass, always brass?

In the early years of LGB, users could choose between the now proven brass rails and cheaper rails made of aluminum. These were, however, not weather-proof and therefore only suitable for indoor use in the long term.

Outdoors they could only be operated for a short period of time and at a temperature above 41°F.

Due to these limitations, they were less than popular with LGB fans: The aluminum rails were therefore discontinued just a few years after the start of LGB. Although LGB tracks were laid frequently and effectively indoors, only the brass ones guaranteed reliable use on garden layouts.



Sturdy and weather-proof: Solid brass rails are still used to manufacture robust LGB tracks.



Competition 2/69: Gold-rush mood in the LGB community.

Going for gold

Everything started with the Stainz. The Stainz was one of the original three available LGB loco versions based on the prototype of the Styrian State Railroads. The Stainz can even be found in the LGB emblem. So it is hardly surprising that the first gold-plated LGB loco was also a Stainz. Back in 1969 a competition run by the young LGB Depesche magazine gave readers the chance to win the golden Stainz model. The aim was to find the LGB fan with the longest installed LGB layout.



Big and loud: The Trans-Harz Railway steam loco 2080S.

First Harz LGB loco

The first LGB model of the Harz narrow-gauge railways was launched in 1973/1974. And it represented further milestones: At the time, the Trans-Harz Railway steam loco 2080S was the first large LGB steam loco. It was also the first steam loco with electronic steam, bell, and whistle sound, which was quite a sensation back in the day.



Mogul (2018D): First "real" LGB model of a typical US steam loco with a 2-6-0 wheel arrangement and built during the pioneering era of North America.

Long awaited

In 1984, LGB unveiled its first prototypical model of a US loco: the "Mogul" steam loco 2018D. This type of model had been long awaited by fans. This also contributed to the loco's huge success: More than 10,000 models of this classic US loco were sold within a short period of time. This outstanding model held a special place in the hearts of the many LGB fans in the USA.

"LGB is a real blessing! The brand inspires ardent garden rail-roaders worldwide to take their hobby to new heights. We look forward to many more years of cooperation and wish LGB all the best."

Stefan Rude,
Marketing and Sales Manager, Gebr.
FALLER GmbH.



BIG IN THE BACKYARD

The letters LGB are the abbreviation of the German phrase "Lehmann Gross Bahn" which literally means "Lehmann Big Railroad". In fact, the German word "Gartenbahn" (garden railroad) was not totally inappropriate given its resistance to the elements. But the LGB inventors did not want to restrict use of the railroad to backyards from the start and consciously kept the options open for all user groups.

THINK BIG

The scale 1:45 was very nearly the gauge adopted by LGB. At least if Eberhard Richter, who had been in favor of the smaller format had got his way. In the end, his brother Wolfgang, was able to convince him of the merits of the "big" format 1:22.5.

BIG IN SAXONY

In 2000, the Saxon IV K, the most produced narrow-gauge steam engine for a state railroad in Germany, was honored with an LGB model. The model was occasionally also referred to as "Wiedervereinigungs-Lok" (the reunification loco) or "Schöne Sächsin" (the Saxon beauty).

MODEL BECOMES PROTOTYPE

At the end of the day, the idea of LGB was highly successful: To mark "75 years of the Glacier Express" in 2005, RhB decided to paint one of its famous brown "Crocodiles" – the Ge 6/6 I 412 – blue to match the dark-blue/cream livery of the salon cars that had been restored some years previously. LGB fans had been using this color combination on their layouts since 2001.

We would like to thank Aldo Farnetti for the support in the illustration of the LGB history. The comprehensive compendium LOK LGB Book by Aldo Farnetti / Paolo Zanin - Photographer Roberto Turci will be presented in detail in the next issue.

Big anniversaries – big trip

In the Club's anniversary year 2018, there are special Club trips to the Rhaetian Railroad, to Saxony, and to the Harz narrow-gauge railways. To celebrate 50 years of LGB, Club members can also opt to join one of the LGB trips.

Where the big ones are at home: During this anniversary year, why not visit the home of various railroads that are among the most fascinating in central Europe. The legendary Harz region is hailed as "the El Dorado of narrow-gauge railways". Narrow-gauge railways in Saxony have a history stretching back more than 130 years. It is the federal state with the most diverse heritage railroad scene in Germany. And no railroad enthusiast should give up

the chance to visit Switzerland with its magnificent routes and the Rhaetian Railroad. Make the most of this opportunity and benefit from the Club offers on these trips to three railroading hot-spots: exclusive special trains with guaranteed window seat, partial participation of the Märklin management and a Märklin employee (Swiss trips), and much more besides. And, needless to say, Club members also receive a 5% discount on all trip prices.



The Weisseritz Valley Railway is believed to be the oldest public narrow-gauge railway in Germany and is only one of many attractions on the trip.

Discover famous places and spectacular routes in Switzerland with LGBTOURS and be inspired by the nostalgic flair of the Saxon narrow-gauge railways.

Combine the impressions of the Saxon metropolis of Dresden and the experience of narrow-gauge railways. From the Zittau narrow-gauge railway to the Fichtelberg railway, one highlight follows the other on this trip in May. In August then, LGBTOURS are heading into Switzerland. Visit the main workshops of the RhB and enjoy the ride on the Rhaetia Pullman Express, hauled by a legendary Crocodile.

Further trip highlights: Guided tours through the various agenda items, partial participation of the Märklin management and a Märklin employee (Swiss trips), an exclusive LGB car as a souvenir.

Travel dates	
May 12 to 21, 2018	Royal Saxon: Seven railroads and large parts of Saxony in ten days
July 29 to August 5, 2018	Swiss dream in historic trains: Albul, Bernina and Arosa lines

Prices, exact itinerary and registration at: **LGBTOURS**, www.lgbtours.de / Tel. +49 (0) 91 03/16 97



In a special anniversary train for the Club members, the trip passes through the breathtaking landscape of the canton of Grisons.

The scenic highlights on the Swiss trip organized by Bahnreisen Sutter will definitely have you reaching for your camera. In the Harz region, retrace the steps of Faust and Mephisto at full steam.

One of Switzerland's most beautiful cantons - Grisons - welcomes the Club members. Experience the "Swiss Grand Canyon", or "Vorderrheinschlucht", from your window seat on board the heritage train. Not only beautiful half-timbered houses, but also the legendary Brocken are waiting on the trip to the Harz. With full steam we head up the Witches' mountain and along the romantic Selke valley.

Further trip highlights: Guided tours through the various agenda items, partial participation of the Märklin management and a Märklin employee (Swiss trips).

Travel dates	
June 6 to 10, 2018 September 26 to 30, 2018	Travel on the special anniversary train through Grisons and at full speed through the Rhine Valley
September 4 to 9, 2018	Harz fairy tales: Full steam ahead up the Brocken, visit Faust and Mephisto on historic trains

Prices, exact itinerary and registration at: **Bahnreisen Sutter**, www.bahnen.info / Tel. +49 (0) 76 52/91 75 81

Good luck! 50 YEARS OF LGB - A NOSTALGIC EXHIBITION IN MARIENBERG/SAXONY (UNTIL MAY 27, 2018)



The 2: Ulrich Franke and son Maximilian are LGB specialists.

A unique item: This bust served as a prototype for the little aviator toy beside it on the right and came from one of the Richter offices.

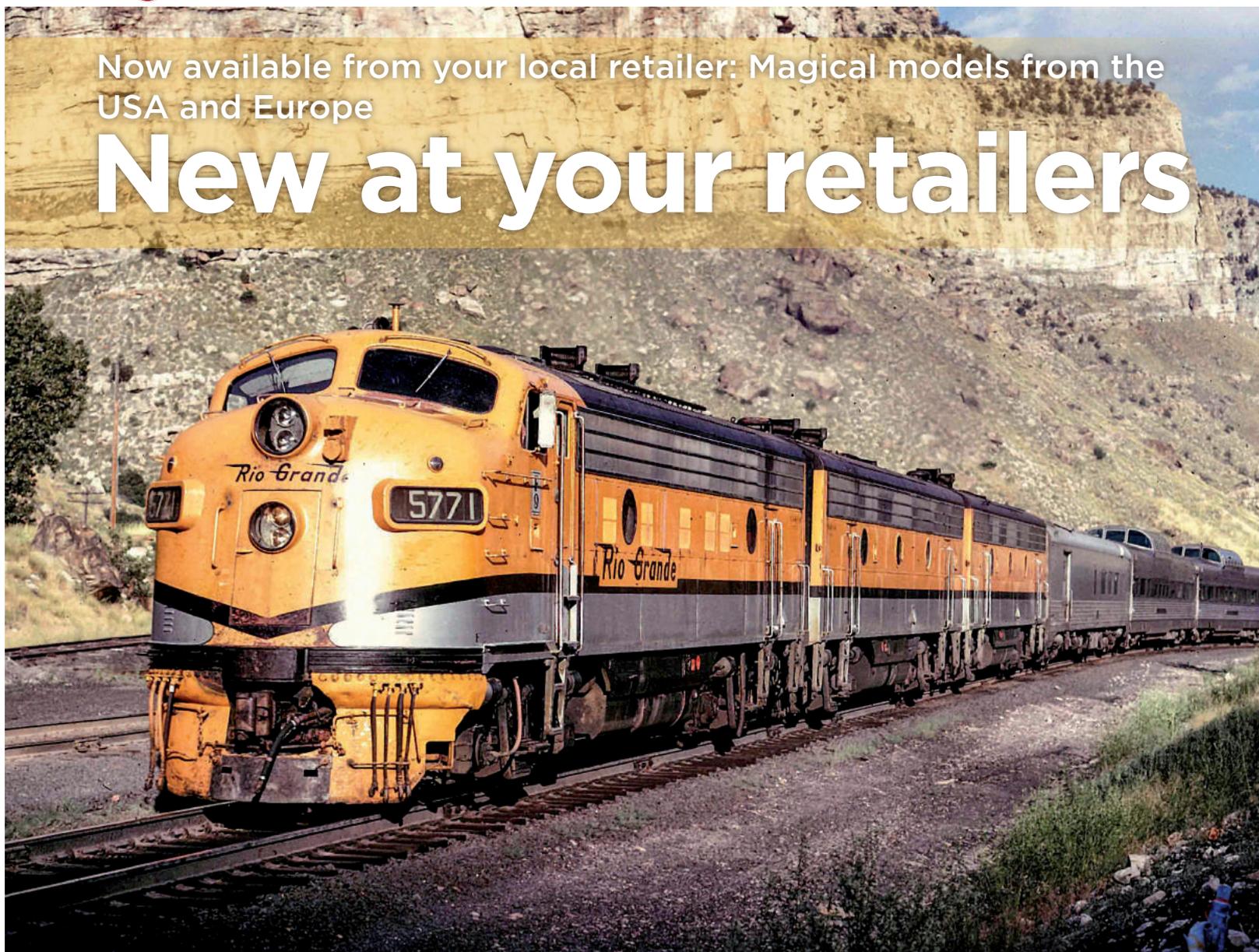
Marienberg is also known as a mining town and the region's mining heritage can be explored at a visitors' mine complete with a conveyor system that used to be powered by horses, the "Pferdegöpel". The top floor of the former administrative building in the district of Lauta (Lauter Strasse 12) is the location for an exhibition from November 4, 2017 to May 27, 2018 to mark the anniversary of Lehmann-Gross-Bahn. Organized by Ulrich Franke and family, it features a huge collection of toys manufactured by Ernst Paul Lehmann Patentwerk throughout its history in Brandenburg and after 1952 in Nuremberg. Advertisements, company documents, and correspondence are also on display. The large number of existing and some extremely rare LGB models is breath-taking. Those who have never seen the first real "Stainz" steam loco will find it there along with an unbelievable variety of other sometimes bizarre toy designs that were released on the market under the name LGB. A functioning "explosion car" on a small indoor layout is sure to go down a bomb with younger visitors in particular. The exhibition is very much designed with families in mind but also includes items that even old LGB fans will certainly never have seen before in person. Opening times Tuesday to Sunday, including public holidays 10:30 a.m. - 4:30 p.m.



A trip down memory lane: Ulrich Franke displays in chronological order the toys that have left the gates of the toy manufacturer to this day.

Now available from your local retailer: Magical models from the USA and Europe

New at your retailers



DRGW F7A Diesel Locomotive Era III, item 20579

- Two powerful Bühler motors with ball-bearing mounts, traction tires
- Light functions such as headlight, Mars light
- Electric socket for connecting the B-unit
- mfx/DCC digital decoder
- Length over buffers 24-1/64 inches

No less than 43 of these type F7 A-unit diesel locos manufactured by EMD operated on the Denver & Rio Grande Western Railroad. The model has a different road number to the A-unit item 20578 and comes with the original paint scheme and printed lettering from era III. Both trucks are powered by Bühler motors with ball bearings. The rear end of the loco is fitted with an electric socket for supplying power from the decoder in the A-unit to the loudspeaker in the matching B-unit.



Grizzly Flats CHLOE Steam Locomotive Era VI, item 23131

- Prototypical paint scheme and lettering
- Both wheel sets are powered, traction tires
- Illuminated headlights
- Length over buffers 11-1/32 inches

It started life as a sugar cane loco, then became the attraction at the Grizzly Flats Railroad, a museum railroad in California, until 2006 – “Chloe” has since then resided in the Orange Empire Railway Museum near San Bernadino. LGB/Märklin is supporting a program of refurbishments to return the loco to full working order. For more information, visit oerm.org (C59 fund). The model comes with an engineer figure. The matching museum railroad cars are the 1st class passenger car (item 30431) and the car set (item 32441).



Grizzly Flats Passenger Car, 1st Class, item 30431

- Authentic paint scheme and lettering
- Metal wheel sets
- Length over buffers 7-31/64 inches

The loco “Chloe” operated until 2006 on the Grizzly Flats Railroad with this observation car and additional open-air passenger cars. The model of the observation car, 1st class, is prototypically painted and lettered. The seatbacks can be reversed as in the prototype so passengers are always facing in the direction of travel. A complete train can be modeled with items 21428 and 32441.



Grizzly Flats Passenger Car Set 2nd and 3rd Class, item 32441

- Prototypical paint scheme and lettering
- Metal wheel sets
- Length over buffers, 7-31/64 inches

The Grizzly Flats Railroad was nothing other than a garden railroad in prototypical scale (914 millimeter gauge) that ran on the property of Disney animator Ward Kimball. To supplement his 1975 observation car (item 30431), he had these “gondolas” for carrying passengers constructed in 1993. The trio was hauled by the loco “Chloe” built in 1907, which he named after his daughter. All train models are absolutely authentic.



SOEG Diesel Locomotive, Road Number Köf 6001 Era VI, item 23591

- All the wheel sets are powered
- Powerful Bühler motor, traction tires
- Engineer's cab doors open
- Length over buffers 11-3/8 inches

The type HF 130 C diesel loco was constructed between 1944 and 1946 and arrived in 2010 via Rügen as Köf 6001 in Bertsdorf. Since then, it has been in service with the SOEG (Saxon-Upper Lusatian Railway Company) on the Zittau narrow-gauge railway. All the wheel sets are powered, it has a prototypical paint scheme and lettering of era VI, and an mfx/DCC sound decoder with many light and sound functions.



DR Steam Locomotive, Road No. 99 653 Era III, item 20480

- Two powerful Bühler motors with ball-bearing mounts, traction tires
- Firebox and cab lighting, smoke generator
- mfx/DCC digital decoder
- Length over buffers 16-9/64 inches

The narrow-gauge steam loco 99 653 (former Saxon VIk), which first entered service in 1920, remained in operation with the GDR State Railroad until 1973 (in Wilsdruff). The model of the class 99.6 of the GDR State Railroad (DR) features the prototypical paint scheme and lettering of era III. It is equipped with an mfx/DCC decoder with many light and sound functions as well as a smoke generator with cylinder steam sounds that are synchronized with the wheels. The cab doors and the smoke box door can be opened.



DR Passenger Car Era III, item 36353 and 36354

- The doors on the platforms can be opened
- Riveted steel walls look
- Length over buffers 22-53/64 inches

The models of the 2nd class passenger car for narrow-gauge railways of the DR feature the prototypical paint scheme and lettering of era III and have different road numbers, fully detailed interiors and metal wheel sets. The VIk (item 20480) is available as the matching loco.



Delivery starting from February 2018

DEV Spreewald Steam Locomotive Era VI, item 24742

- Prototypically painted and lettered
- mfx/DCC digital decoder
- Integrated smoke generator
- Limited to 499 units worldwide
- Exclusive wooden case with booklet
- Length over buffers 12-6364 inches

The 100-year-old DEV “Spreewald” steam locomotive is still in service on the museum railroad from Bruchhausen-Vilsen to Asendorf. The model comes with a paint scheme and lettering reflecting how it currently looks, and is equipped with a ball-bearing-mounted Bühler motor and traction tires. The built-in mfx/DCC decoder has numerous sound and light functions. The model is exceptionally detailed and is fitted with etched metal loco plates.



RhB Salon Car Era VI, item 33520

- Highly detailed interior
- Opening doors, metal wheel sets
- Length over coupling 25-6364 inches

The model of the As 1161 salon car of the RhB is the perfect companion to the Alpine Classic Pullman cars, item 36658 and 36659. The paint scheme and lettering are prototypical for the Alpine Classic Pullman Express of era VI. Many separately-applied details and the interior lighting guarantee authenticity.

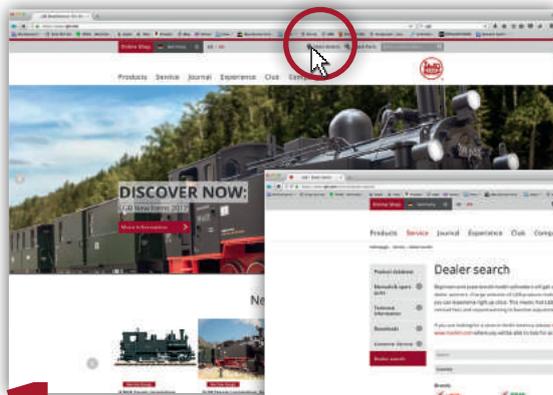
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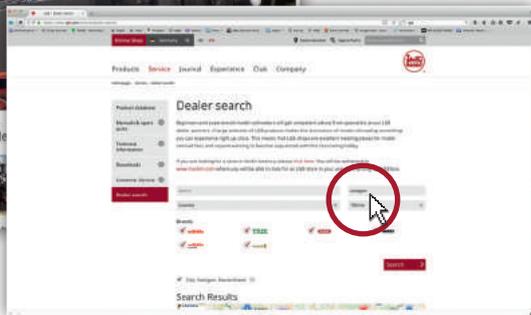
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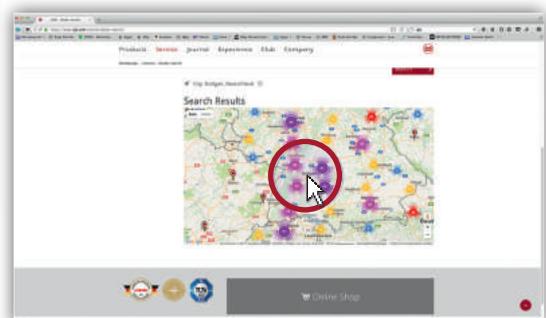
1 Call up the start page

www.lgb.com

Click on the menu Store locator



2 Enter zip or city



3 Select dealer and print adress

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New cooperation partners

The Pressnitz Valley Railway in Jöhstadt and Stellwerk S in Herrenberg

In May, everyone on the LGB anniversary trip to the Saxon narrow-gauge railways will experience the station festival of the Pressnitz Valley Railway. But the Interessengemeinschaft Preßnitztalbahn e. V. in Jöhstadt also runs narrow-gauge locos on the Pressnitz Valley Railroad all year round - see www.pressnitztalbahn.de under "Fahrplan" (Schedules). Good for Club members: The IG Preßnitztalbahn association has now become an official Club cooperation partner. Club members receive a discount on the price of the day ticket on presentation of their Club card (19 euros instead of 20 euros).



In 2017, the heritage railroad through the Pressnitz Valley celebrated its 125th anniversary and is one of the destinations on the LGB anniversary trip to Saxony in May 2018.

The exhibition of the huge N-gauge model layout of Stuttgart stations by Wolfgang Frey is now partially open, together with the canteen "Gleis 16", and is our latest Club cooperation partner. Club members pay an admission fee to Stellwerk S of 5 euros instead of 6.50 euros. The commitment of initiator Rainer Braun and his comrades from the MEC Herrenberg and the cooperation partner N-Club International has paid off: The model buildings from the 1980s have been authentically restored in detail and the N layout offers lively running operations. You can find Stellwerk S in 71083 Herrenberg, Germany, Nagolder Str. 14, south of Stuttgart on the A81 highway. For information, see www.stellwerk-s.de (e-mail: info@stellwerk-s.de). Times of opening: Thursday to Sunday: 10 am to 6 pm, admission until 5 pm.



Dispatcher wanted

There is no better instructor for the hobby dispatcher than the Club cooperation partner "Förderverein Lehrstellwerk Kornwestheim" - Tower's Supporters' Association (www.lehrstellwerk-kornwestheim.de). One of its specialties: A hobby dispatcher's diploma, which can be gained by attending a respective course. The next course takes place from May 10 to 13, 2018. The Club is drawing three participation cards for the training course among all members. You can apply online in the club area at www.lgb.com (club) under the heading "Gewinnspiel" (Competition) or by postcard (Gebr. Märklin & Cie. GmbH, LGB Club, Postfach 960, 73009 Göppingen; Stichwort "Hobby-Fahrdienstleiter"). Closing date for participation is March 31, 2018. In the case of a written application, please provide your name, address and Club membership number. The winners will be notified by us in writing.



The technical facilities of "Museum für Eisenbahnbetrieb" (Museum for Railroad Operation) have been protected by a historical preservation order since 1995 and are also used by "Förderverein Lehrstellwerk" (Tower's Supporters' Association) to hold hobby dispatcher's courses.

Men at work

The winners of the photo contest for the club gift 2017

Last year, all LGB Club members received the Freight Set from Pola as a gift. In issue 3/2017 of LGB Depesche, the Editorial team – as has become tradition – announced a photo contest for the club gift. Readers were asked to put their creative energy to great use and to produce and capture a lively and detailed scene on their layouts using boxes, barrels, and pallets. Customizing the freight items and adapting them to the style of the layout was not only allowed but also encouraged; audacious use of the gift welcome. We were as surprised as last year about where entries were sent in from. Besides Austria and Germany, LGB fans from as far afield as Australia, Canada, the Netherlands, and Sweden took part, although our Dutch friends were by far the largest contingent. The three winners have now been picked and since everyone made every effort to send in large, printable images this time round, we can finally present their ideas in a befitting manner.

Rev. Dr. Vern Cracknell from Lower Mitchem, Australia, even sent us a detailed description that we decided not to withhold from our readers, especially as Vern makes his own figures and gives each one a name. He was awarded 1st prize for his particularly atmospheric installation (image down right) and will receive the RhB Gondola with sound (item 33353). Although Vern will undoubtedly replace the band with some of his own model friends. The 2nd prize – a RhB Bicycle Car (item no. 34555) – was awarded to Keith Yundt from Victoria, Canada, for his heavy-load scene, and the 3rd prize – the RhB High-Side Gondola (item 40881) – went to Peter Baetsen from Venlo in the Netherlands for his farm shop that has received an LGB delivery.



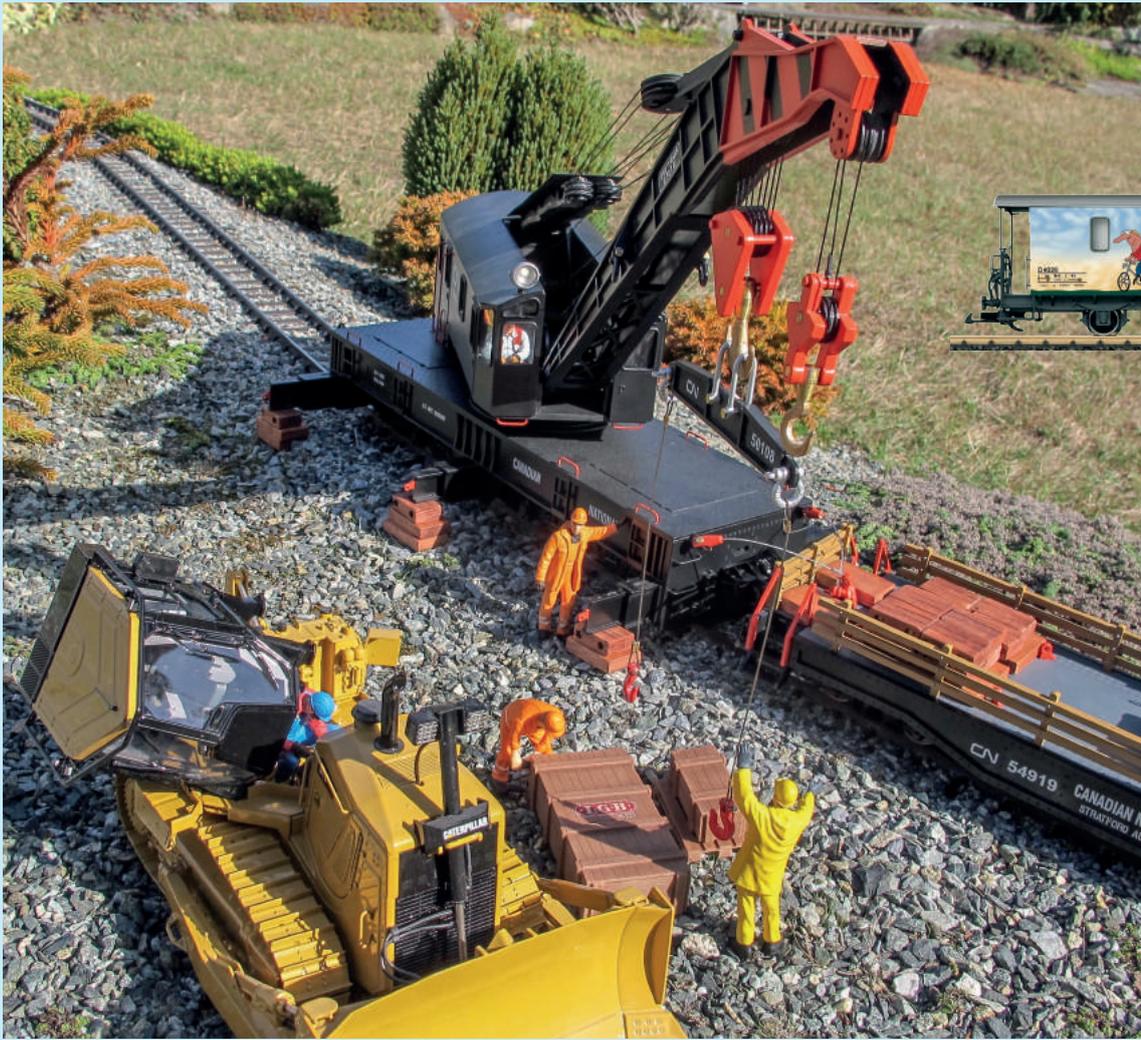
1st prize: RhB Gondola with sound (item 33353)

Vern Cracknell describes his Club gift installation:

The scene is on the Kangaroo and Cockatoo exhibition layout. The field locomotive has shunted the flat wagon into the Burr and Tooth sawmill spur line, and has moved onto a passing loop to await its next task. The wagon is loaded with crates of new machinery required for this mill, as well as for others in Burr and Tooth's chain of mills. The yard crane is being employed for unloading, and Sid (in the yellow/green check shirt) and Reg (in the grey singlet), partly hidden on the other side, are providing the muscle power. Brian is on the landing to guide the first crate onto the deck. Barry, with the hat and about to drink from his water bottle (as the day is in the mid 30s) is the engineer who will install the new machinery, and he watches hoping that all will go well. Bert works on the adjoining goods platform positioning barrels and drums for later pick up.



1st place: Vern Cracknell came first in the Club gift photo contest 2017 with his down under still-life. Note: The freight boxes have even been decorated with labels. All further elaborate details can be found in the box above.



2nd prize:
RhB Bicycle Car
(item 34555)

2nd place: Canadians don't really do things in half measures. Heavy machinery is being used on the layout of Keith Yundt from Canada to unload large spare parts for the bulldozer that luckily broke down right next to the tracks. The huge crane is the model of a 250 t Bucyrus Erie steam powered crane that has been converted to diesel power. It is fully remote controlled, including the lights and digital sound.



3rd prize:
RhB High-Side
Gondola
(item 40881)

3rd place: The heavy freight items look out of place in the idyllic backyard landscape. And the body language of Peter Baetsen's worker as he tries in vain to unload the large box from the stake car says it all. Further, he gives the impression of not being absolutely certain about the actual destination of the load. Is it really meant for the farm shop?

PHOTOS: VERN CRACKNELL, KEITH YUNDT, PETER BAETSEN

Exclusively for members: The Club model 2018

A FREIGHT CAR DELIGHT FOR CLUB MEMBERS: A FOUR-AXLE STAKE CAR OF THE FICHELBERG RAILWAY WITH A FULL LOAD OF LOGS.

LGB Club model 2018

SDG/DR stake car, era VI (item 41591)

- 1 Prototypically painted and lettered as a museum car of the SDG/DR
- 2 Wood cargo, chains to secure the stakes included
- 3 Metal wheel sets
- 4 Length over buffers 16-11/32 inches

People need warmth: This showpiece of every narrow-gauge freight train supplies you with essential firewood for your stove during the cold season. Any railroad paparazzi out there with a penchant for the Fichtelberg Railway will surely know that its prototype can be seen on a regular basis to this day in photographs of freight trains with varying loads on the Harz narrow-gauge railways. Individually applied stakes make this model an additional highlight on every garden railroad.

Notes on ordering

Every Club member can order one example of the exclusive Club model from their LGB retailer using the order form enclosed in issue 4/17 or their Club card. Please note: The order forms are not transferable.

Important

Whether you order using the form or with your Club card, closing date for orders for the Club car is **April 30, 2018**. Deliveries are scheduled to start in quarter 3, 2018. At the end of 2018, it is planned to send out personalized Certificates of Authenticity directly to purchasers. **Item 41591**

PHOTOS: KÖTZLE



IMPORTANT G-GAUGE DATES FROM FEBRUARY TO MAY 2018

Start into the season

Once again, the new year kicks off with numerous events and meetings to usher in the upcoming garden railroad season. Here, we provide an overview of key dates for G-gauge fans for outdoors and indoors.

UP TO MAY 27

Special exhibition "50 years of LGB" in the mining town of Marienberg

To mark the 50th anniversary of LGB, the collection of Ulrich Franke and his family will be on display in the "Pferdegöpel" in the mining town of Marienberg until the end of May. The exhibition features numerous models, in chronological sequence, from former LGB employees and passionate collectors. These exhibits showcase the company's exciting history. Also on display are sheet-metal and plastic models produced at the Lehmann factory after 1952.

"Pferdegöpel" at the Rudolph Pit in Lauta Lautauer Hauptstrasse 12 09496 Marienberg, Germany www.marienberg.de

MARCH 09-11

Faszination Modellbahn, Sinsheim

Year after year, Faszination Modellbahn is one of the top meeting places for well-known manufacturers and model railroaders in all gauges. That's because many new releases make their debut at the trade fair and are presented to the public for the first time. Naturally, it also provides yet another opportunity to admire the numerous extravagant layouts.

Messe Sinsheim GmbH
Neulandstrasse 27
74889 Sinsheim, Germany
www.faszination-modellbahn.com



APRIL 01/02

Easter excursions with the DEV.

Just in time for Easter, the popular diesel rail car trips to Heiligenberg organized by the Deutscher Eisenbahn Verein will

take place once again. The route passes through the Vilsler Forest. Children of all ages can look forward to an exciting treasure hunt at the end of the trip, where the Easter Bunny will have hidden a few surprises. Tickets must be booked in advance at tickets.museumseisenbahn.de or by phone on +49 (0)4 21/36 36 36.

Deutscher Eisenbahn-Verein e.V.
Bahnhof 1
27305 Bruchhausen-Vilsen, Germany
www.museumseisenbahn.de

APRIL 19-22

Intermodellbau Dortmund

The world's largest model building and model sport exhibition once again provides an opportunity to exchange information, observe the market and acquire all kinds of models and accessories. Also on offer is a varied supporting program with numerous workshops and varied special presentations, competitions, and show interludes.

Messe Westfalenhallen Dortmund GmbH
Rheinlanddamm 200
44139 Dortmund, Germany
www.intermodellbau.de

APRIL 20

Special steam train on Walpurgis Night



Let yourself be carried away to the world of witches and devils. On Walpurgis Night, the special steam train brings its guests to the great Walpurgis celebration at Stieger See. A welcoming drink of witches' potion provides plenty of inner warmth. This will be followed by an authentic witches' dance conducted in a romantic campfire setting at the lake to banish the winter and welcome the spring. At around midnight,

the "Demons Express" departs for the return journey to Gernrode (Harz).

Freundeskreis Selketalbahn e. V.
Gernrode (Harz)
Otto-Franke-Strasse 2
06485 Quedlinburg, Germany
www.selketalbahn.de

MAY 1

Steam-up 2018

At the beginning of May, the garden railroaders in Hamm will host yet another "Steam-up" to mark the start of the new season. Suitably spruced up and polished, the garden railroad gets back on track after the winter break. In keeping with tradition at this event, visitors can look forward to a few surprises, from special guests to new or revised sections of track.

Hammer Modell- und Gartenbahnfreunde
Maximilianpark Hamm
Alter Grenzweg 2
59075 Hamm, Germany
www.hmgf.de

MAY 19-21

Grand celebration "50 years of LGB" on the Pressnitz Valley Railway

This year, the traditional Jöhstadt station festival at Whitsun is devoted to the anniversary "50 years of LGB". With numerous visiting vehicles on-site, you can experience the unique appeal of the appearance of models and their prototypes side by side. On the Pressnitz Valley Railway, you can experience busy railroad operations late into the night with the Saxon I k train, a Saxon IV k and VI k, the Mh 52 and the V 51.

Interessengemeinschaft
Pressnitztalbahn e. V.
Am Bahnhof 78
09477 Jöhstadt, Germany
www.pressnitztalbahn.de



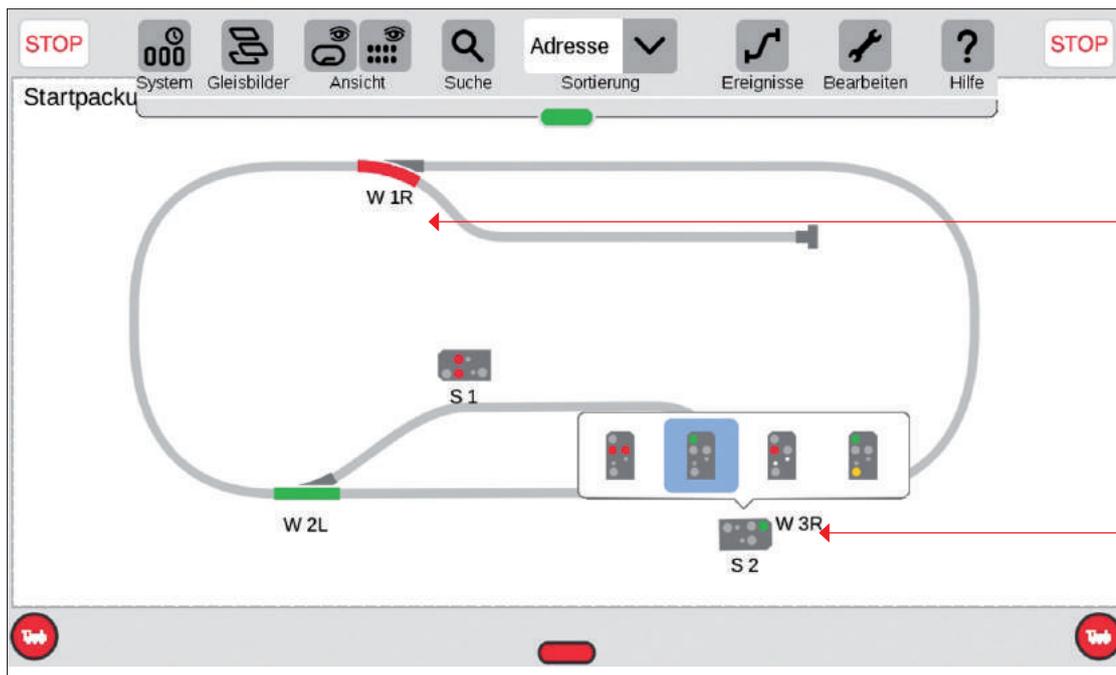
Control with extra convenience

The new track diagram control screen concept in Central Station 3 provides for exceptionally convenient control and switching on the layout. In Part 6 of our series of articles, we also show you just how easy it is to transfer data from CS2.



The track diagram control screen is the heart of the layout control system. It allows the model railroader to replicate a part of or the entire layout. Central Station 3 presents this functional area with unprecedented clarity and power: It is now possible to create track diagram control screens that replicate the actual route of the model railroad tracks with a high degree of accuracy - thanks not least to the system's new ability to dis-





Make sure that the STOP button is not active. The tracks must be fed with current before they can be switched.

Tap the turnout to change its setting.

A tap of the finger changes the signal aspect or displays the switching options.

Switching items conveniently at the tap of a finger: The pictograms display the respective switch position.

play curved sections of track. Once the model is “in the bag” so to speak, it can be used to control all railroad traffic on the layout.

It also puts you in command of a fully functional signal tower: You can activate solenoid items such as turnouts, signals, lamps or other components directly in the track diagram control screen simply by tapping briefly on the corresponding symbols. The current switch position is displayed in color: for a turnout, red for the “curved” frog, green for the “straight-ahead” setting. The setting of signals is also easily recognized in this way. If the signal or turnout has more than two switch positions, CS3 displays all options at the tap of a finger – simply tap again to select an option. It could scarcely be easier. Only two prerequisites must be fulfilled for switching by hand to function: The edit mode must not be activated; if it is, the background of the track diagram is shown entirely in white (see the image above). And naturally, the solenoid items must be supplied with current before they can be operated. Therefore, the STOP button should not be pressed.

The zoom function, which allows you to reduce or enlarge sections of the track diagram at will, is very useful. To do this, drag your thumb and first finger together on the display (to reduce the view) or apart (to enlarge the view).

Those who are familiar with the use of a tablet PC or a smart phone will use this function straight away by intuition.

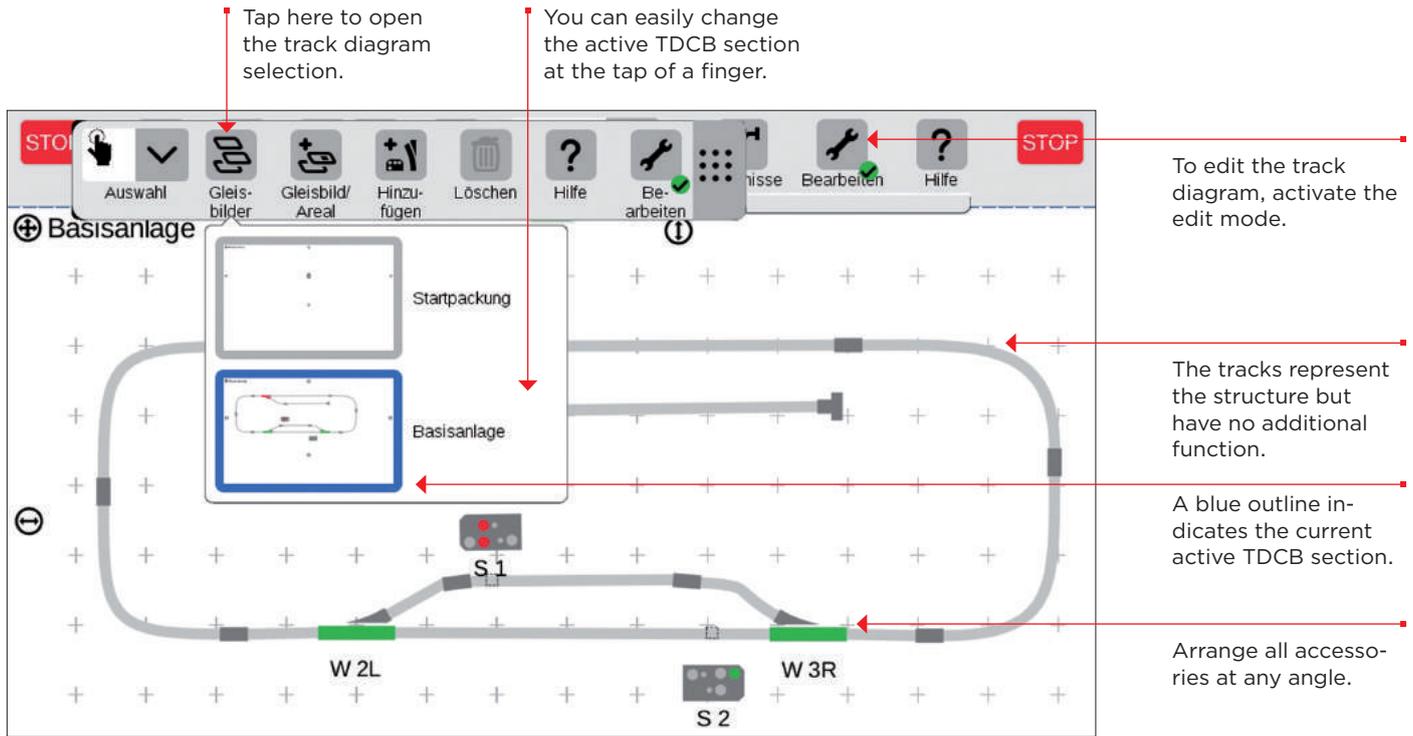
The more extensive the model railroad layout, the more you will appreciate the benefits of the Track Diagram Control Board (TDCB) concept of CS3. That is because this concept allows individual parts of the layout such as stations, staging yards or switching areas to be clearly presented in separate TDCB sections. All TDCB sections are stored above and beside one another on the screen background and you can move them by swiping your finger. Navigating

TIP

When developing new devices, Märklin goes to great lengths to preserve their compatibility with previous generations as far as possible. This also applies to the popular option of transferring existing loco and accessory data from CS2 to CS3 (see “Transferring CS2 data” on the opposite page). For technical reasons, however, there is one exception in relation to track data: The previous CS2 layouts cannot be used in CS3. This is because the track diagram control screen concept of CS3 and its enhanced display capabilities are too different to the CS2 layout.

between different track diagrams is also very straightforward: To bring a TDCB section containing the desired track diagram section to the foreground, simply tap on a visible part of the relevant TDCB section. Double-tap to display the TDCB section in full-screen mode. Alternatively, use the menu at the upper edge of the screen: Use the “Track diagrams” button to switch between the TDCB sections: The menu displayed shows a compact graphic view of all available track diagrams and TDCB sections - with the active TDCB section highlighted by a blue outline (see page 55, image at top). Simply tap on the TDCB section entry that you want to move to the foreground. The selection menu is then automatically hidden and the required track diagram displayed. You can now get on with editing or switching this part of your layout.

One particularly useful feature for larger track diagrams is the “View” function, which displays the respective solenoid item types in color on the track diagram thereby highlighting them – while the other elements remain gray. As a result, you can quickly gain a good overview, even of larger track diagrams. The CS3 therefore comes highly recommended as a reliable assistant – after all, every model railroader welcomes some extra clarity when performing the role of dispatcher. ►



Switching between different track diagram sections is easy: In the menu, tap on "Track diagrams" and choose the TDCB section.

Transferring CS2 data

With its introduction, the Central Station 3 replaced its predecessor the Central Station 2 as flagship of the Märklin control units. Depending on the scenario, it still makes perfect sense to continue using the CS2. However, if you have previously used a CS2 to control your garden railroad and now want to take advantage of the added control possibilities the CS3 offers, you certainly won't want to discard the data painstakingly entered on your CS2. The good news is that since Märklin attaches great importance to ensuring compatibility, you can transfer all your valuable loco and accessory data to the new control center in just a few steps.

To do this, plug a USB stick containing a backup of your CS2 data into one of the two USB ports on your CS3. Then tap "System" in the toolbar on the extreme left upper edge of the screen and, on the startup page that opens, tap "System" again at the bottom left. This takes you directly to the CS3 system settings. There, you tap the "Restore" button to start the data transfer process (image below). CS3 now asks if you want to continue - confirm by tapping on the check mark. This opens a file selection dialog in which you select "USB" in the

left-hand column and then tap in the adjacent column on the directory containing the CS2 backup. Now select the backup file by tapping it and confirm with "OK". The CS3 now reads in the data and restarts. After a few moments, the starting screen of the CS3 will be displayed, with all loco and item data from the CS2.

Tap the "Restore" button in the "CS3" section in the system settings to start the data transfer.



www.rhb.ch/berninaexpress

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Shop window railroad with a proud tradition



For 26 years, Jörg and Ulrike Pfeifle have transformed the shop window of their drugstore “Pelikan-Apotheke” into a railroad landscape for the Christmas season. The Christmas motif for 2017 was the Turtmann Valley in the Swiss mountains.

Pharmacist and model railroad fan Dr. Jörg Pfeifle first hit on the idea in 1966. He decorated the shop window of his drugstore in Stuttgart with a model railroad. During the holiday season, he set up his old Märklin gauge 1 steam locomotive from the pre-war era and several cars in the shop window and added various products from his drugstore. However, the promotional effect did not live up to expectations, so he altered the concept. He kept the railroad but removed the products from the cars.

Following the emergence of LGB at the end of the 1960s, LGB locos and cars started to appear in the drugstore window. Based on his assertion that “Christmas

“Christmas time is railroad time, and that goes for the shop window too”

time is railroad time”, he set up an LGB loco and several cars shortly before the start of Advent in the 23-foot-long shop window of his drugstore “Pelikan-Apotheke”, which he took over in 1973. In 1978, he set the trains in the shop window in motion. He began with an ÖBB diesel loco 2095, which commenced railroad operations on single-track route. The loco equipped with two Bühler motors was

better suited to the demands of continuous operation in the shop window than the single-motor LGB locos in Pfeifle’s fleet at the time. The risk of a loco getting stuck or needing some downtime forced him to set up a second track on the shop window to accommodate an LGB train. “This meant that if one train came to a stop, at least the other train could keep running”, he explains today with a smile.

An automatic circuit box offers greater convenience

The locos travel back and forth in shuttle operation. Back in the early days, there was no such thing as an LGB automatic



Dr. Jörg Pfeifle (81) is a model railroader, an LGB enthusiast since 1969, and the owner of an LGB indoor layout. Every year since 1991, he and his wife Ulrike Pfeifle have constructed a model railroad landscape for Christmas in the show window of their drugstore "Pelikan-Apotheke". The couple are a well-rehearsed team: He is responsible for the track layout, the rolling stock and the installation, while she attends to the design of details and the landscapes and paints the backdrops.

circuit box. Instead, Pfeifle used a switching device that he built himself and installed at the terminal stations. Control was initially provided by a control unit from a Faller traffic light system for freeways. Jörg Pfeifle subsequently added components from the LGB turnout motor with extra switches. The LGB automatic circuit box has been installed on both tracks since 2010, with Pfeifle setting the circuit boxes to the "simple" operating mode. The sudden braking is not noticeable on trains that run slowly anyway.

Touch the window to start the trains

Pfeifle made another technical modification in 1984. In the left part of the shop window, he mounted a contact switch on which the viewers could place a hand to start the trains. The current feed is interrupted after a preset operating time, and can be reactivated when a hand is placed on the switch again. The solution

The terminal station Moosalp: The town of Moosalp with its train station is the final destination of the twin-track route on the right-hand side of the shop window.



The three-dimensional effect: The right-hand side of the shop window has a depth of 4-17/64 feet. A village as well as a small area of woodland complete with sawmill has been set up here.

came from a pilot production series by the Fischertechnik brand of construction toys, which were sold by Noch. It functions with the help of photoelectric cells. Even 33 years later, it functions reliably and is very popular both with the older and especially the younger spectators, who can send the trains on their way at the touch of a hand. The control system also reduces the strain on the rolling stock – an important consideration for Jörg Pfeifle. Instead of running for twelve hours per day, seven days per week for no less than nine weeks, the trains now only operated when onlookers were in-

terested in seeing them move. The result: The stress on motors, wheels, conductor pickup shoes and other wear components was significantly reduced. This had a positive impact. "Since then, the trains operate with far fewer problems", recalls Jörg Pfeifle.

Creating railroad scenery

It was during the pre-Christmas season of 1991 that Pfeifle first added some railroad landscape to the shop window. His wife Ulrike Pfeifle, a keen amateur painter, contributed ideas for the landscape design and painted the 29-17/32-foot-long backdrop for the rear wall of the drugstore window. Since then, new railroad landscapes in the form of an artistic backdrop are created every year – and this year marks the 27th Christmas shop window design. The theme for the first ▶

Painted landscapes: Ulrike Pfeifle not only designed the mountain scenery on the back panel, but also painted the mountain climbers who are attempting to scale the new peak.



year was a landscape from the mid-west of the USA with impressive cliffs. This was followed in 1992 by a Swiss theme inspired by the area around the Morteratsch Glacier and Lake Sils. In 1993, it was a steam locomotive shed and Esslingen, a wonderful town near Stuttgart. The themes of the USA, Switzerland and Germany also inspired the basic motifs and rolling stock in the ensuing years. Different landscapes were created every time. "It would be boring to use the same motif each time", explains Jörg Pfeifle, referring both to the spectators as well as the layout constructors Jörg and Ulrike Pfeifle themselves. Year after year, they pondered motifs and highlights for new layouts. A curved trestle bridge, the Brusio spiral viaduct, a circus, a cable car, a roundhouse loco shed, a main train station, a sawmill, a burning house – all have featured in previous layouts. The painted back panels are mostly inspired by specific prototypes, although a certain degree of imagination is applied to extend the landscape to fit the 29-17/32-foot format of the back panel. Suitable buildings are sourced from the couple's own collection or from friends. On some occasions, items loaned by Vollmer found their way into the drugstore window.

Motif 2017: The Turtmann Valley in Upper Valais, Switzerland

Preliminary discussions of the Christmas railroad for 2017 began as early as spring 2017. Photos of the Turtmann Valley gave the Pfeifles the idea for the new motif. The concrete preparations got under way at the end of October. The backdrop panels from previous years were inspected. A section of the back panel of the Turtmann Valley motif from 2004 was suitable, as were two parts of the back panel of the Morter-

A steep incline: Behind the train station at Pontresina, the terrain rises steeply. Several groups of mountain climbers are attempting the ascent.



Limited space: At its narrowest point, the shop window is just 20-55/64 inches deep

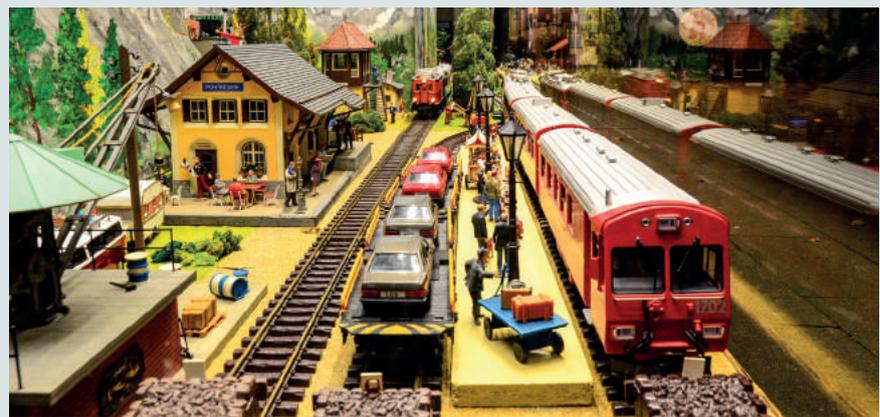
BUILDING TIP 1

Offer participation options: On a shop window layout with running trains, there is a great deal to discover. It is even more exciting if the layout lets onlookers give the signal to depart themselves. Children are particularly fond of using this facility. However, the journey intervals should not be too long. The shorter they are, the sooner the onlookers can set the rolling stock in motion again. An operating time of three minutes has proven to be a good compromise.



BUILDING TIP 2

Slow-running trains: Due to the confined space in shop windows, it is not usually possible to run trains on continuous loops. Instead, they travel back and forth between two terminal stations. These routes are generally short, as is the operating time. To ensure that the trains remain in motion for as long as possible, they should run slowly. However, they must also be traveling at sufficient speed to ensure that the automatic circuit box is reliably triggered.



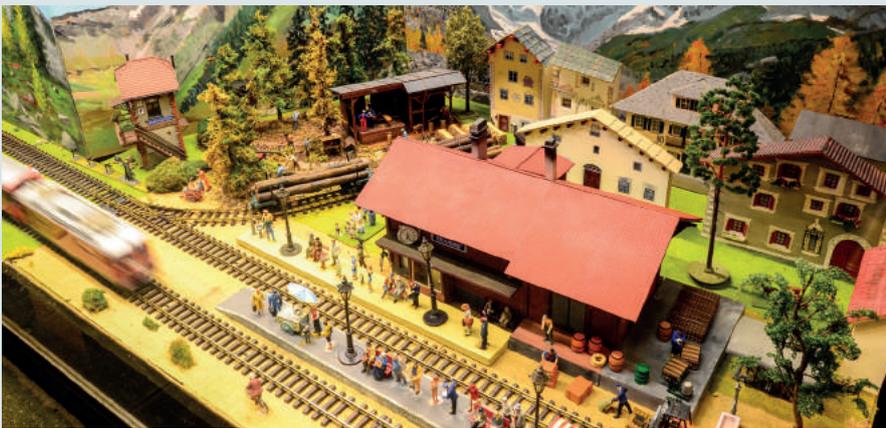
BUILDING TIP 3

Avoid excessively long trains: Long trains with numerous cars are more interesting and impressive than short trains. However, train lengths are restricted due to the limited space available in the shop window. The trains should be as long as possible, but short enough to enable them to run for a short distance in the shop window. The 23-foot-long track in the drugstore shop window provides sufficient space for the RhB Ge 2/4 electric loco with two four-axle salon cars to make its journeys back and forth.



BUILDING TIP 4

Low-relief buildings save space: There is little space in the shop window for complete buildings. Low-relief buildings are an interesting alternative that take up less space and allow groups of houses to fit in the shop window. Complete buildings can then be used to greater effect for highlighting individual structures such as a train station.



BUILDING TIP 5

More is actually more when it comes to figures: A shop window layout should offer viewers lots to look at. This applies to the painted backdrops, the scenes, the buildings, and the figures used. The more figures there are dispersed throughout the layout, the more there is to explore. The same applies to the use of passenger cars.



atsch motif of 2016. Minor visual changes were all that was required for them to be used in 2017 as well. Two entirely new sections were created, on which Ulrike Pfeifle painted additional mountain landscapes. The individual scenes were then prepared. The group of mountain climbers that was supposed to be suspended from the mountain face required a great deal of attention. The figures by Prehm were dressed in knee breeches and hats in the style of the 1930s and did not fit into the contemporary Swiss mountain landscape. Jörg Pfeifle modeled helmets on the climbers' heads, Ulrike Pfeifle painted them and updated their clothing. Needles were also inserted into their hands and feet so that they could be securely anchored to the cliff face.

The creative setup phase

The setting-up process in the shop window began as usual on the weekend before the first Advent. Individual smaller parts such as the platforms with passengers are assembled in advance, however, the access to the shop window is too narrow for larger sections. Jörg Pfeifle designs the layout in situ in the shop window. He draws on many years of experience in designing his drugstore window and his model railroading expertise, although his advancing age is somewhat of an impediment. "It's at times like these that I notice that I am 81 years old. But I still really enjoy setting up the layout in the window."

The old LGB cable car

However, not everything runs smoothly. The old LGB cable car was intended as a highlight for the new layout. After a great deal of preparatory thought on how the mountain and valley stations could be accommodated within the confines of the shop window and how to ensure that everything ran properly, a problem was discovered with the cable propulsion mechanism. It did not function as planned and resisted all efforts to repair it. To avoid jeopardizing the scheduled inauguration of the layout before the first of Advent, Jörg Pfeifle ceased all repair attempts and designed the other parts of the shop window railroad. At the valley station of the mountain cable car, he then set up a large number of construction workers who were attempting to get the old motor up and running again. None of this fazes any of the spectators. Even without the functioning cable car, there is plenty to discover in the Christmas shop window 2017 of the drugstore.

Two anniversaries – 50 years of LGB in 2018 and, in 2017, 125 years of steam locomotive no. 2, or the “Stainz”, of the Styrian State Railroads – are reason enough to concern ourselves with the well-being of the loco that appears on the LGB emblem. It is often the case that after an initial phase as a garden railroader (usually with a starter set), the small steam loco is displaced by larger, more elaborate machines and is destined to spend most of its days on the siding. In short: Everyone has one, hardly anyone uses it regularly. Yet, at the start of the LGB journey this loco was incorporated into the brand logo, especially because of its short dimensions and the symbolic representation of the narrow-gauge railway. Since then, both the loco and the brand have become inseparable in the minds of most fans. The prototype of our “Stainz” was a two-cylinder wet steam loco with a B wheel arrangement. It had a service weight of 11.6 tons and measured just 18 feet in length. It was therefore the smallest narrow-gauge steam loco used for public transport services in Austria and was built by Krauss of Linz in 1892 and

delivered to the Styrian State Railroads (STLB) with serial no. 2774 for use on the Preding-Wieseldorf-Stainz line.

To ensure the garden railroad “Stainz” is positioned in a favorable light and a typical steam loco environment, we are going to construct a small rural locomotive station this anniversary year. The corresponding individual construction suggestions will be demonstrated on a layout module within suitable sur-

Major task for one of the world’s most beautiful steam locomotives: The

roundings so as to describe their exact function. The end product will be a real showpiece as opposed to a useless dust collector. According to their needs, readers can adopt the suggestions and recommendations and fit them into their existing layouts, rather than having to follow them strictly. Our arrangement and yet to be supplied track plan fol-

low special requirements whose inter-relationships will be explained in due course. Topics such as module, building, and track construction will obviously also be addressed.

Some of the first questions you want answered when considering construction are about the available space and the model situation; in other words, where the locomotive station will be located on the assumed prototype line. In our case, the finished module is to measure 5.2 x 2.6 ft to possibly even fit into the grid of an existing indoor layout. The prototype situation should be based on the end point of a branch line, but, in terms of the track plan, also be suitable for continuation of the line (something not unknown on prototype lines).

Prototype-based: The “Stainz” layout

The site itself features various pieces of technical equipment, first and foremost a water crane and a coaling plant, but also a shed for tools and sand, an inspection pit, a slag removal facility, a turntable, and a small loco shed, as well as the many small features that are not

The “Stainz”, the LGB emblem, is finally getting a home. In four parts, Karsten Speyer will show how to create a rural locomotive station along with all the essential accessories. So here we go:



The “Stainz”

obvious at first glance.

In our mind’s eye, we envisage the following picture: The central point is a manual turntable covered with planks of timber. Seeing as water has to be refilled more often than coal, a water crane should be situated on the access track. This would ensure it can also be reached without having to use the turntable. On a feeder track to the turntable we have planned a coal stockyard with a raised platform. Depending on the actual locomotives being operated, a coaling platform that guaranteed ease of access to the coal boxes of the locos used to be installed. In the next issue, we shall construct a staggered version that is suitable for the “Stainz” and the rear coal boxes of the U class of the Mur Valley Railway, as well as the 99 6001 of the Trans-Harz Railway. The inspection pit will also be incorporated in this section of track along with the facility for cleaning the smokebox and for removing slag from the firebox. The loco shed will also have its own feeder track. The smallest version for overnight storage of one loco should be sufficient, but it can be optionally extended to house two locos next to each other (more expensive due to the extra turntable connections) or one behind the other (normal arrangement in the past and cheaper). A free space for

firing up or shutting down a guest loco can also be set up, just like a redundant freight car with a loading ramp for tools, quite often also used as a coal or timber storage facility. Conceptual designs and a track plan will be presented in the next issue.

Our initial project: The water crane

In this issue, we shall begin with the

and connectors measuring 1/2 inch in diameter. A brass union piece forms the base that will be attached to a wooden or plastic baseboard. The vertical pipe is available in standard lengths. This is followed by a T-piece onto which an end cap is bonded or soldered. The boom, or goose-neck, which ends with a 90° angle is attached to the center connection. The height of the vertical pipe is determined after loosely assembling the in-



construction of a component that is a necessity on every steam loco layout: A water crane. Its constituent parts can, as always, be purchased from a hardware store, this time in the plumbing section. We are using copper fresh water pipes

dividual parts according to the highest filler opening of the locos (Fig. 1).

The goose-neck must reach from the planned installation point to the filler opening of the loco but should not be longer than this; simply test it out ▶

Typically “Stainz”: In the LGB starter sets, as shown here with a freight train (item 70403), the small steam loco is the unrivaled powerhouse.



PHOTOS: LGB, WOLF ROTH; TEXT: KARSTEN SPEYER

z” diorama

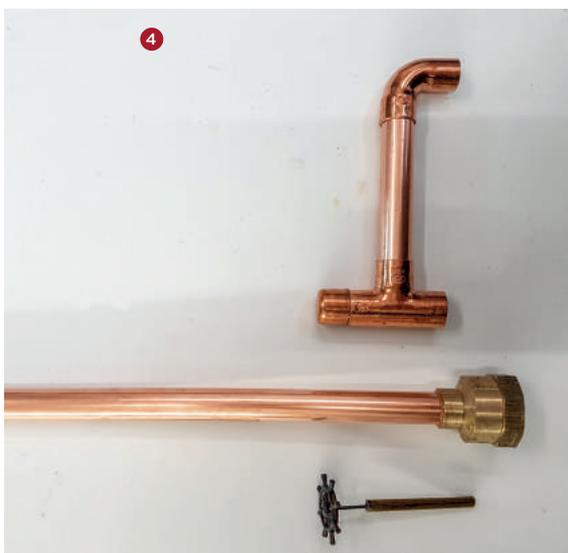


and, if necessary, cut the pipe sections to length using a pipe cutter (Fig. 2).

With long booms, a strut can be positioned between the boom and the vertical pipe to avoid bending, but in our case this is not really necessary. Some track sections also featured double-joint water cranes, if the railroad had sufficient fi-



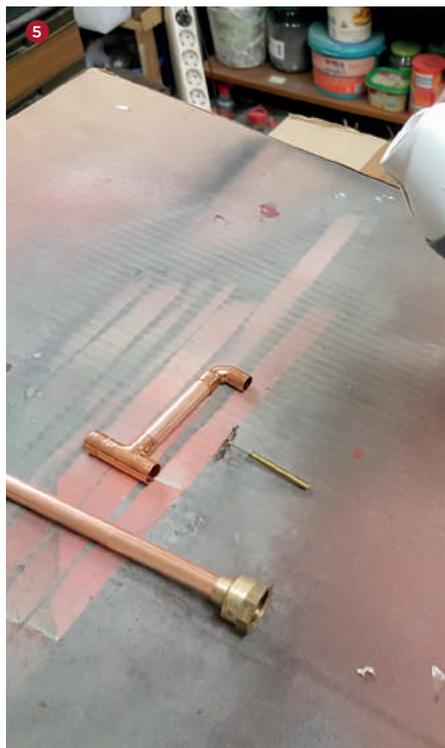
ancial resources. On this type of crane, the boom can be swiveled due to two additional brackets, allowing engineers to stop the loco at various points. Finally,



a hand wheel (from a model ship) is installed in a short section of pipe to turn the water supply on and off (Fig. 3).

If you do not fancy soldering, you can also use contact adhesive (Fig. 4). Once the parts have been installed, they need to be coated with primer and then lacquer (Fig. 5).

After drying, glue a piece of fabric around the end of the goose-neck to create a bag-hose spout, which can be



hung in the water tank or, in the case of the "Stainz", in the filling funnel. In the

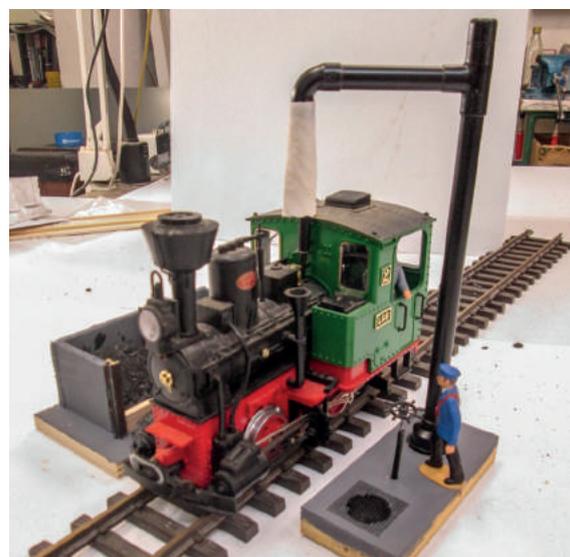


Bit by bit: During the anniversary year, Karsten Speyer will create an idyllic steam landscape based around the "Stainz" loco.

baseboard, use an appropriate center bit to mill a hole for the foot of the crane and another one (holes are not through holes, drill press) underneath the idle position of the spout, as well as a small hole for the hand wheel (Fig. 6).

The baseboard can be painted to resemble a concrete plinth, thick acrylic paints or tinting concentrates help to add structure. Finally, install the water crane and the hand wheel. A drain grate made using a piece of fly screen the idle position of the boom gives the component an authentic look. In the vicinity, also directly next to the track, use clear lacquer to create puddles, and in the winter install a metal fire basket next to the vertical pipe to prevent it from freezing.

Coming up in the next issues, we will look at the track plan and more construction suggestions (e.g. coal bunkers Fig. 7) which together form a complete unit. However, they are meant to show just one of the many opportunities available and to help you get your creative thinking caps on in order to provide our "Stainz" and its fellow locos with a suitable home. In the next issue: Layout plan and coal bunkers.



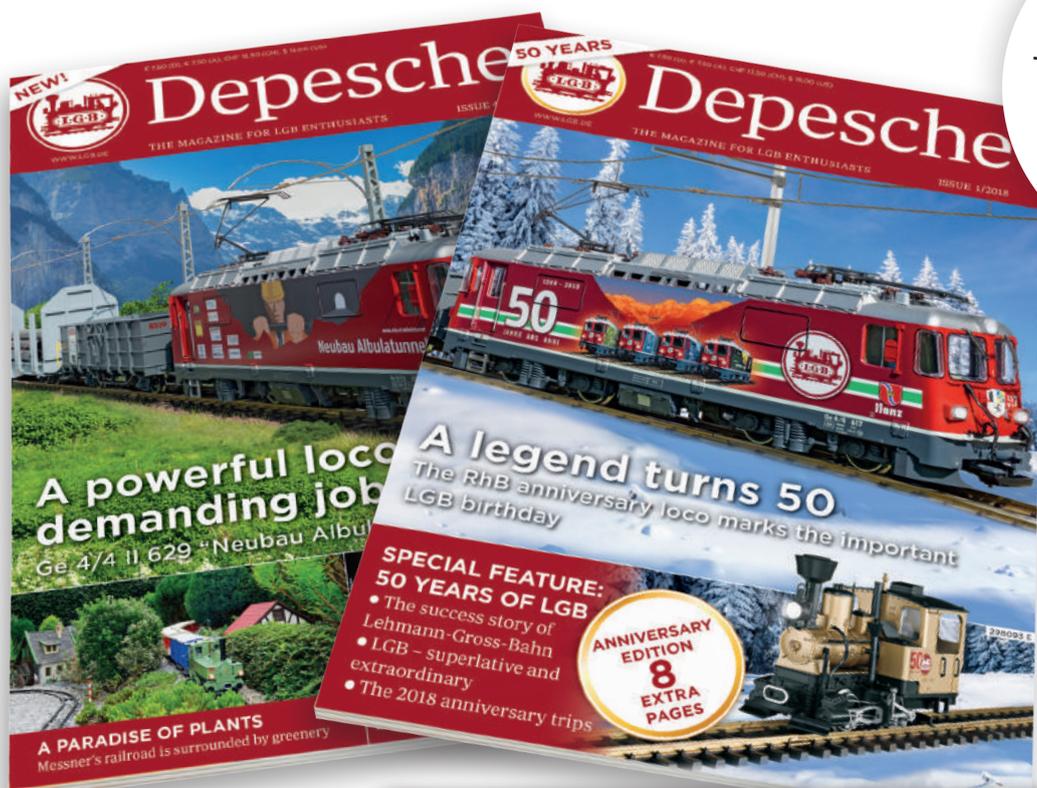
50 YEARS



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How do I calculate track lengths for outer radii?

I want to lay two semicircles with a radius of $47\frac{1}{4}$ and $54\frac{21}{64}$ inches respectively using flex track. By how many inches must I shorten the inner rail to ensure that both rails are the correct length to allow connection to the next pieces of track?

This is one of those surprising mathematical conundrums where this difference in length between the two semicircles is solely dependent on the difference between the radii. The inner rail has a radius r_1 . In G-gauge, the radius r_2 of the outer rail is $\frac{1-49}{64}$ inches bigger than the radius of the inner rail. Therefore, we can write this as follows: $r_2 = r_1 + \frac{1-49}{64}$ in. The semicircle is calculated from $r \times \pi$. This means that the outer rail has a length of: $r_2 \times \pi = (r_1 + \frac{1-49}{64} \text{ in}) \times \pi = r_1 \times \pi + \frac{1-49}{64} \text{ in} \times \pi = r_1 \times \pi + \frac{5-9}{16}$ inches. Consequently, the outer rail in G-gauge is always $\frac{5-9}{16}$ inches longer, regardless of the radius of the inner rail. And this is the length by which we must shorten the inner rail on the semicircle using flex track.

How fast should my LGB loco actually be capable of running?



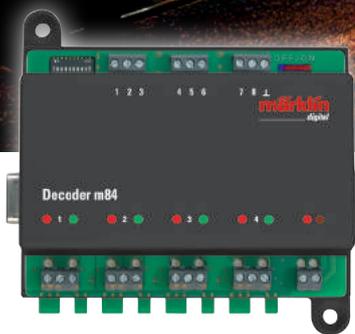
The question as to whether a loco is running too fast or too slow is of course a highly subjective one, to begin with. Those who want to assess its performance should therefore start by comparing the speed of the model with that of the prototype. The first step here is to measure the speed of the model. If we assemble a small oval layout comprising twelve curved tracks of $R 1 = 23\frac{5}{8}$ inches (equivalent to a diameter of $47\frac{1}{4}$ inches) and four straight tracks with a length of $11\frac{13}{16}$ inches, this layout has an overall length of $16\frac{13}{32}$ feet. Using a clock with a second hand, we now check how long it takes the loco to complete ten laps of the layout - ten laps in order to minimize measurement errors. For example, if the loco takes 50 seconds to complete these ten laps, this results in a speed of $(16\frac{13}{32} \text{ feet} \times 10 \text{ laps}) / 50 \text{ seconds} = 3\frac{9}{32}$ feet per second. Expressed in miles per hour, a value of $3\frac{9}{32}$ feet per second = $11811\frac{1}{32}$ feet per hour = $2\frac{15}{64}$ miles per hour. Since the prototype is 22.5 times bigger than the model, this equates to a speed of $50\frac{21}{64}$ mph.

In the past, model railroad locomotives were generally designed to run extremely fast. In many cases, the only limiting factors were the centrifugal forces, which had to be kept low enough so that the loco would not be hurled from the tracks when it negotiated curves at speed. Nowadays, there is a growing preference for designs that also keep the maximum speed of a model loco in proportion to that of the prototype. However, there are also garden railroaders who like to run all their locos at comparable speeds.

To ensure that these different requirements can be satisfied, current digital decoders such as the mfx/DCC decoder 55028 allow the maximum speed of the loco to be adjusted to suit individual preferences. And this is not only possible for digital operation using CV 5 (the configuration variable that sets the maximum speed), but also for analog operation using CV 177. If you are not satisfied with the maximum speed of a loco that is fitted with a modern LGB decoder, you should start by checking the settings of these two variables. However, you will need a suitable digital controller that permits access to these parameters. Nevertheless, your specialist dealer will certainly be only too happy to help you fine-tune your model's performance to suit your preferences.

► Please send your readers' questions by e-mail to LGB-Depesche@3g-media.de or by post to LGB Depesche, c/o Gebr. Märklin & Cie. GmbH, Stuttgarter Strasse 55-57, 73033 Göppingen, Germany.

How can I switch lamps on my LGB railroad on and off in digital operation?



m84 Decoder
(60842)



LGB Decoder
(55025)

There are two different ways of doing this. You can either use an m84 Decoder (60842) by Märklin. This has four independent switches that can control four corresponding powered areas with lamps. In this case, power can be supplied from a separate AC or DC power source so that the power requirements of the lamps do not place an additional burden on the digital system. In our example, we are using a switched mode power pack 66360 by Märklin. However, you could also use any alternating voltage transformer or switched mode power pack with a suitable supply voltage to power the lamps. As an alternative, you can also use an m83 Decoder (60832) or an LGB Decoder (55025). While the Decoder 55025 will be familiar to LGB railroaders, the use of Decoder 60832 in this role will be a novelty for many LGB fans. For this reason, we want to present it in closer detail:

To perform this task with the m83 Decoder, you set mode 2 in CV 79 by programming the value 2. This component now has eight outputs that can be switched on and off independently via eight addresses. Each of these outputs can deliver a current of up to 200 mA. If the m83 Decoder is set to decoder address 1, for example, then these eight outputs are activated by eight switches using DCC accessory addresses 1 to 8. If decoder address 5 is set, the lamps are activated using DCC addresses 17 to 24. As a result, the complete m83 Decoder can supply a current of up to 1.6 Amps. If this output is drawn directly from the digital system, we recommend switching the power supply to the switched mode power pack 60822 by Märklin and an LGB switched mode power pack 51090. The lamps do not then place an added load on the digital system. Please note that the outputs of the m83 Decoder supply direct current. Therefore, you must check that the polarity is correct for LED lamps.



From left to right:
Dominik Timpert,
Michael Timpert,
Pascal Hallemann

“It’s a family hobby”

Michael Timpert from Eschershausen in Lower Saxony in Germany is an RhB fan, a founding member of the local garden railroad meeting, and president of what is probably Germany’s youngest LGB club.

We visited the Ith.

“Come on Friday as I fear there will be too much happening on Saturday”. Michael Timpert (at the center of the photo), the driving force behind the IG LGB Freunde Ith, was already looking forward with excitement to the garden railroad exhibition in Lower Saxony that he originally initiated when the editorial staff called to arrange their visit. It was the 16th model railroad show on August 12 and 13, 2017. And since the club is regarded as an institution on the garden railroad scene, there is always so much going on there that Michael Timpert must divide his attention between several areas. So Friday it is, which unfortunately means continuous rain – although this scarcely bothers the garden railroaders from the collective municipality of Eschershausen-Ith during the photo shoot. It transpires that the weather around the eponymous Ith low mountain

range to the north can sometimes be extremely wet. If this causes flooding, the little stream that runs along the Timperts’ garden can quickly end up lapping at the outer reaches of the layout. The layout is “to all intents and purposes a private layout”, explains the club president. “It was constructed mainly by my father – although he is no longer as involved for health reasons – by my son (Dominik, left in the photo) and by Pascal (Pascal Hallemann, right in the photo). The others take part primarily in order to run trains”.

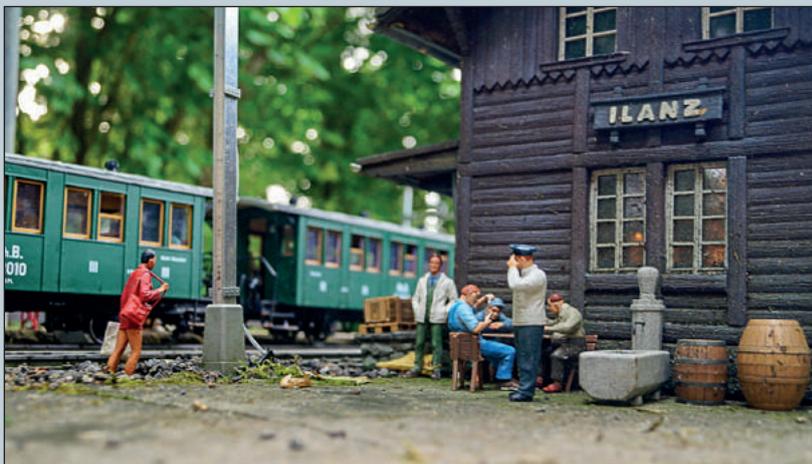
The average age is 25

What sets the LGB Freunde Ith apart: The average age is very low compared with other clubs. “The youngest is seven and the oldest 70, my father. I am the second-oldest (born 1970), actually no, Helmut Jesse (65) is before me. Then there is Ralph Eger (38)

Christian Laube (35), Sebastian Jorczik (25), Pascal (22) and the rest range from seven to 15 years old”, sums up Timpert. A rough calculation reveals that the average age in this club is just under 25 years – some twenty years below the national average in Germany. And Michael Timpert is working hard to maintain this figure. For last year’s garden railroad meeting, he approached several schools under the slogan “Freizeitgestaltung ohne Handy + Co” (leisure activity without cell phones) and invited considerably more than 1,000 schoolchildren to the “GaBa Treffen Süd-Niedersachsen” (garden railroad meeting in southern Lower Saxony). All of them were admitted free of charge. But don’t worry, you haven’t missed out – the campaign will be repeated for the 17th garden railroad meeting in 2018. What makes the garden railroad so interesting for the younger generation? “That is a very diffi-



Timpert's garden: The openly accessible RhB layout not only adheres to its Swiss prototype in terms of mountainous terrain, but is also beautifully laid out.



An appropriate detail for the anniversary: Ilanz station is the namesake of the RhB Ge 4/4 II 617 loco "Ilanz", which is celebrating the LGB anniversary in Switzerland.



Full-time job: Michael Timpert is happy that his family actively helps him maintain the vast and luxuriantly planted garden railroad.

cult question to answer. From a layout perspective, the obvious thing to do is to give everyone a locomotive controller, and let them walk through the garden as they control a train. We take a different approach. Each person stands at a station, controls the trains and then sends them on to the next station. Somewhere or other, there are pre-defined handover points. From then on, the next 'dispatcher' assumes responsibility and passes the trains on. Switching duties adhered to prototypical practice, which involves delivering through cars and removing other cars from the train. Operators must think on their feet at all times and cannot simply tune out because the approaching trains are announced wirelessly. This highly prototypical operation is much more interesting for young people than simply running after trains. And ultimately, it is much more fun".

Who tends the plants? "That's actually a full-time job. Throughout the summer, I work on the layout every day, otherwise it would be impossible to stay on top of things. Thankfully, I can call on the support of my family". Asked whether he set out to recreate the Swiss theme in his garden, this RhB fan responds "It's not actually my garden. My parents bought the property 18 years ago. I now live here, but I was allowed to use the garden from the outset", says the man who received his first LGB starter set at the age of ten.

Continuous operation with wheelbarrows

"In the beginning, there were more than 30 fruit trees here. Once we had removed the wood, there was just a meadow, but the first of my acquaintances to become interested thought the idea of a garden railroad was fantastic and lent a hand. Our long-standing club member Ralph Düe in particular spent every spare minute continuously heaving wheelbarrows through the garden. Without his help, I would never have made such swift progress. This is how our interest group was formed. After four months of construction, we organized our first open day, actually just to show the neighbors what we were doing. More than 100 people showed up. So that's how the garden railroad meeting came into being". Why does he devote so much energy to his hobby? "I fell in love with LGB. It is a family hobby because the railroad is in the garden, which means that daddy doesn't have to disappear into the attic as he would with other gauges ... and my son is even more obsessed than I am". Interested? Then visit www.lgb-freunde.com

ENJOY THE SUN, THE RUNNING FUN AND GARDEN RAILROADS ALL TOGETHER

In good company

Doing things together is simply more enjoyable: more and more LGB enthusiasts meet up in Germany and across the borders to share the inspiration of their super garden railroad hobby with others – including exciting exchanges of experience. Many of them even join forces and come to events in groups. So that other LGB enthusiasts can join them and to make possible lively exchanges with one another, you will find on this page contact details for the LGB enthusiasts circles.

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Publisher
Gebr. Märklin & Cie. GmbH,
Stuttgarter Str. 55 - 57,
73033 Göppingen, Germany
Managing Director: Wolfram Bächle, Florian Sieber

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Kocarnik, LGB, LGB Freunde Ith, LGB Freunde
Rhein/Sieg, Sven Linden, Märklin, Messe Sin-
sheim GmbH, Modelleisenbahnfreunde Eiger,
Hagen v. Ortloff, Film „Heidi“ Praesens-Film
1952, Ramessos, Rhätische Bahn, John Rogers,
Arnulf Schäfer, Shutterstock.com, Alexander
Strobel, Frank Swiergiel, Westfalenhallen Dort-
mund GmbH, Keith Yundt

Distribution
Press Up GmbH, LGB-Depesche Leserservice,
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Tel.: +49 (0) 40 38 66 66-313
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Print
Gmähle-Scheel Print-Medien GmbH,
Esslingen, Germany
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The LGB Depesche is published quarterly. Members of the LGB Club receive the magazine automatically. Individual issues cost 7.50 euro and may be ordered through Press Up (see left). All subject to alteration and availability of all products mentioned. All rights reserved. Reproduction, complete or in part, prohibited except with written consent. This applies also to storage in electronic databanks and reproduction on CD-ROM. The editorial office accepts no responsibility for unsolicited manuscripts, photographs and illustrations.

298093 E

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THE MAGAZINE FOR LGB ENTHUSIASTS

ISSUE 1/2018

+8 PAGES
LGB HISTORY
PART 2

Top model

The steam locos of “Rasender Roland” (Racing Roland), operated by Rügensche Bäderbahn, are the attraction on the line running from Göhren to Putbus on the largest German island of Rügen. The model of the era VI, tender steam loco 99 4652 of RüBB is powered by two motors – one in the tender and one in the loco. Look forward to this Baltic Sea legend. A special appearance of its prototype is planned at the LGB jubilee festival in Jöhstadt (Pressnitz Valley Railway) at Pentecost.

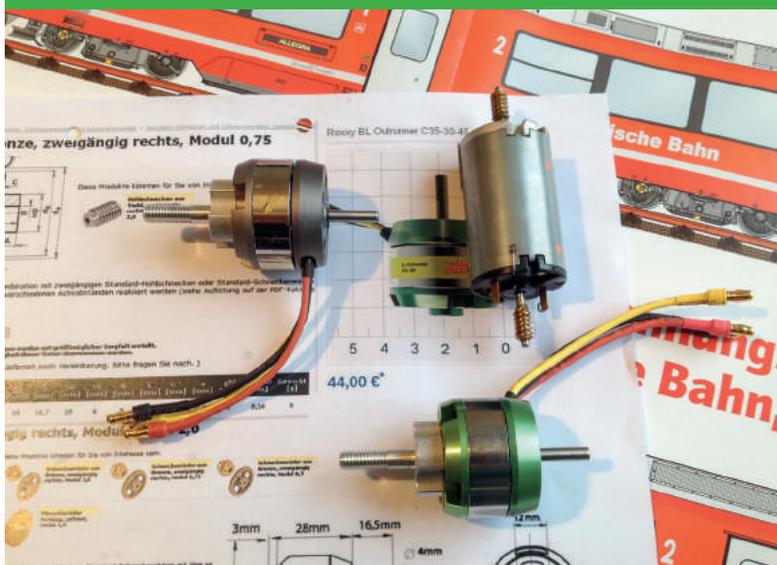


Top layout

Frank Siwergiel from Bremen loves island railroads and has installed one in his basement. The prototype: The narrow-gauge railways on the German island of Wangerooge. It remains the primary form of transport on the island and is operated by the German Federal Railroad (DB). It is therefore the last remaining narrow-gauge line in their network. The tallest building on Frank's island railroad is the red and white Wangerooge lighthouse.



Construction report



Finished at last! Ottmar Görgen's three-phase Allegra. You no doubt remember his steam rotary snowplow Xrot mt 95403 in issue 1/17, which was marveled at by visitors to last year's International Model Railroad Exhibition (IMA) in Göppingen. We now present the construction of his latest scratch-built model – with a real three-phase drive system, just like its prototype.



Note:
We reserve the right to hold articles over to a later issue without notice.

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